

Wicklow Port Access Road Link

Report to Inform Environmental Impact Assessment
Screening Determination

Wicklow County Council

Project number: 60647343
606473434-ZZ-RP-ACM-EN-0003-P01 EIA Screening Report

October 2024

Quality information

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1 Introduction

1.1 Purpose of the Report

This Environmental Impact Assessment (EIA) Screening Report to inform the EIA Screening Determination has been prepared by AECOM Ireland Limited (AECOM) on behalf of Wicklow County Council (WCC) (hereafter referred to as the 'Applicant') for the Wicklow Port Access Road Link (PARL) Project (hereafter referred to as the 'Proposed Development') located in the Key Town of Wicklow, within the administrative area of WCC. The Proposed Development includes the provision of a new pedestrian and cyclist bridge over the Dublin to Rosslare Line at Wicklow Train Station with associated structures such as access stairs and lifts, a new car park and set-down area north of the train station, new cycle tracks and footpaths, and other associated ancillary works. The location of the Proposed Development and the context of its surrounding environs are illustrated in Figure 1.1. The extent of the Proposed Development is hereafter referred to as the 'site'.



Figure 1.1: Overview of the Proposed Development Location and Surrounding Environs

This report to inform the EIA Screening Determination looks to establish whether the Proposed Development necessitates the undertaking of a full EIA and subsequent publication of an Environmental Impact Assessment Report (EIAR) as required under Directive 2014/52/EU (the "EIA Directive") and consider the Proposed Development under Schedule 5 of the Planning and Development Regulations 2001 (as amended) and Section 50 of the Roads Act 1993 (as amended).

Based on Schedule 7 of the Planning and Development Regulations 2001 (as amended), this report contains:

- A plan sufficient to identify the land;
- A description of the Proposed Development, including in particular:
 - A description of the physical characteristics of the Proposed Development and, where relevant, of demolition works;
 - A description of the location of the Proposed Development, with particular regard to the environmental sensitivity of geographical areas likely to be affected;
- A description of the aspects of the environment likely to be significantly affected by the Proposed Development;

- To the extent the information is available, a description of any likely significant effects of the Proposed Development on the environment resulting from:
 - The expected residues and emissions and the production of waste, where relevant; and
 - The use of natural resources, in particular soil, land, water, and biodiversity.
- Such other information or representations as the person making the request may wish to provide or make, including any features of the Proposed Development or any measures envisaged to avoid or prevent what might otherwise have been significant adverse effects on the environment.

This report should be read in conjunction with all other reports submitted with this planning application.

1.2 Qualifications & Experience

This report has been prepared by an Environmental Consultant with AECOM with over two years' experience in environmental consultancy. They have carried out EIA Screenings for active travel schemes and roads projects and assisted in the preparation of EIA Scoping Reports and EIARs for energy and transportation projects. They are a Graduate Member of the Institute of Environmental Management and Assessment (IEMA) (GradIEMA).

The report has been reviewed by an EIA Associate Director within the AECOM Republic of Ireland Environment and Sustainability team responsible for management, coordination, and delivery of a diverse range of projects throughout Ireland, UK and Europe. They have broad experience of managing and delivering solutions for both small and large-scale projects within the environmental sector.

This report has been lead verified by a Technical Director with AECOM. They are a Chartered Environmentalist (CEnv), a Member of the Charter institute of Ecology and Environmental Management (CIEEM) with over 20-year experience of EIA across multiple sectors.

2 Legislation and Guidance

EIA requirements derive from Council Directive 85/337/EEC of 27 June 1985 on the assessment of the effects of certain public and private projects on the environment as amended by Council Directive 97/11/EC of 3 March 1997, Directive 2003/35/EC of 26 May 2003 and Directive 2009/31/EC of 23 April 2009. These were codified in Directive 2011/92/EU of the European Parliament and the Council on the assessment of the effects of certain public and private projects on the environment. Directive 2011/92/EU was subsequently amended by Directive 2014/52/EU of the European Parliament and of the Council of 16 April 2014. Together, these comprise the EIA Directive.

The EIA Directive had direct effect in Ireland from May 2017 and was transposed into Irish planning law in September 2018 in the form of the European Union (EU) (Planning and Development) (Environmental Impact Assessment) Regulations 2018. The regulation sets out the amendments made to a number of Irish acts and regulations in line with the EIA Directive (as transposed into Irish legislation). This includes amendments to the Planning and Development Act 2000 (as amended) and the Planning and Development Regulations 2001 (as amended). The Planning and Development Act 2000 (as amended) and the Planning and Development Regulations 2001 (as amended) provide guidance as to the specific requirements for both public and private projects to assess their potential effects on the environment and the steps to be undertaken in relation to whether a full EIA (in the format of an EIAR) is required.

Under the Planning and Development Regulations 2001 (as amended), EIA development fall into two Schedules. EIA is mandatory for developments listed within Schedule 5, Part 1, while Schedule 5, Part 2 developments require a full EIA if they are a development of a type set out in Part 2 of Schedule 5 which equal or exceed, a limit specified within Schedule 5 Part 2 in respect of the relevant class of development.

Criteria to evaluate whether significant impacts on the receiving environment will arise from a proposed development are listed under Schedule 7 of the Planning and Development Regulations 2001 (as amended). A list of the relevant information to be provided by an applicant or developer for the purposes of sub-threshold EIA Screening is presented in Schedule 7A of the Regulations.

Additionally, the Roads Act 1993 (as amended) sets out EIA requirements for roads projects and has been amended to take account of the requirements of the EIA Directive in line with the European Union (Roads Act 1993) (Environmental Impact Assessment) (Amendment) Regulations 2019. Annex III of the EIA Directive is specifically referenced in Section 50(1)I of the Roads Act 1993, as amended, to be considered when identifying any potential likely significant impacts of a project.

2.1 Other Relevant Guidance

This report was also cognisant of the following guidelines:

- Section 3.2 of the Environmental Protection Agency (EPA) *'Guidelines on the information to be Contained in Environmental Impact Assessment Reports'* (EPA, 2022);
- Office of the Planning Regulator (OPR) (2021), *'OPR Practice Note PN02 Environmental Impact Assessment Screening'*;
- Department of Housing, Local Government and Heritage (DHLGH) (2020), *'Guidance for Consent Authorities regarding Sub-threshold Development'*;
- DHLGH (2018), *'Guidelines for Planning Authorities and An Bord Pleanála on carrying out Environmental Impact Assessment'*;
- European Commission (EC) (2017); *'Environmental Impact Assessment of Projects: Guidance on Screening'*; and
- WCC (2022), *'Wicklow County Development Plan 2022 – 2028'* (hereafter referred to as the 'CDP').

3 Methodology

As set out under the relevant legislation, there are three key steps when carrying out an EIA Screening for a particular development, as follows:

- **Step 1** is to determine if the proposed works represent a development as understood by the EIA Directive and if a mandatory EIAR is required. Such developments are defined in Article 4 of the EIA Directive and set out Annex I and II of the Directive, Schedule 5 of the Planning and Development Regulations 2001 (as amended) and Section 50 of the Roads Act 1993 (as amended) where applicable.
- **Step 2** is to determine whether the development exceeds a specific threshold as set out in the Planning and Development Regulations 2001 (as amended) Schedule 5, Part 2 – Development for the purposes of Part 10 (the only type of development to which thresholds do not apply are those considered to always be likely to have significant effects and therefore require an EIAR).
- **Step 3** is to determine if the development is likely to have significant effects on the receiving environment, taking into account the criteria listed under Schedule 7 of the Planning and Development Regulations 2001 (as amended). There are no exacting rules as to what constitutes “significant” in terms of environmental impacts. The responsibility is on Planning Authorities to carefully examine every aspect of the development in the context of characterisation of the development, location of the development and type and characteristics of potential impacts. It is generally not necessary to provide specialist studies or technical reports to complete this screening process, rather to investigate where further studies may be required, and where risks, if any, to the integrity of the receiving environment may lie.

4 Site Location & Context

The Proposed Development is located in the Key Town of Wicklow, within the administrative boundary of WCC. The location and overall context of the Proposed Development are illustrated in Figure 4.1.

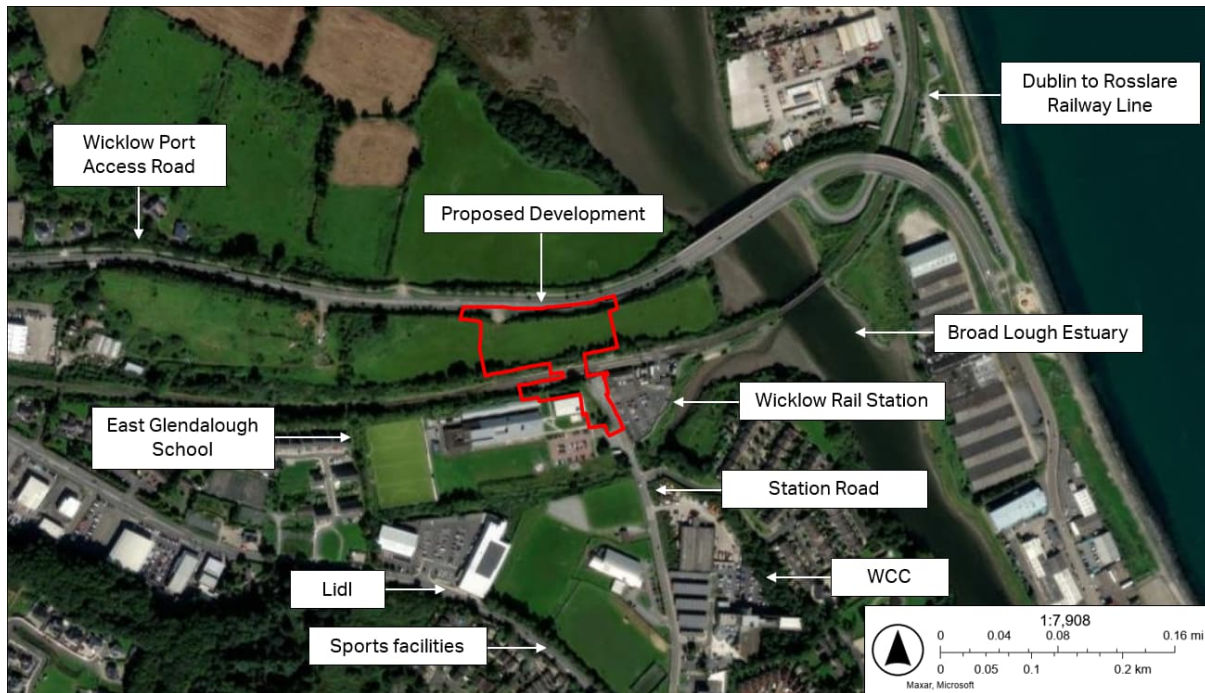


Figure 4.1: Proposed Development Location and Context

4.1 Site Description

The Proposed Development site extends over an area of 1.34 hectares (ha) which includes the northernmost section of Station Road (approximately 60 metres (m)), green spaces along the northern and eastern boundaries of the East Glendalough School, and a greenfield area between the Dublin to Rosslare Railway Line and the Wicklow Port Access Road (also known as the R999 Regional Road or Drumkay Road) which is bordered on the north and south by trees and scrub.

Station Road is a cul-de-sac that begins at the junction of the R750 Regional Road and extends for approximately 430m, providing access into the Wicklow Train Station. There are footpaths on either side of the road, but no cyclist facilities present. There is currently no direct access from the train station or Station Road to the Wicklow Port Access Road.

4.2 Surrounding Environs

North of the railway line, lands adjacent to the Proposed Development comprise greenfield areas with boundary or scattered vegetation, and are zoned for 'Enterprise and Employment'. The site is bordered to the north by the Wicklow Port Access Road which provides connection to an area east of the Broad Lough Estuary which contains the Wicklow Port, commercial and enterprises properties, residential areas, educational facilities, and other community services.

South of the railway line, lands adjacent to the site comprise the Wicklow Rail Station to the east and the East Glendalough School to the west. Further south along Station Road (between 45 to 340m, depending on the location), there are WCC offices and facilities, the Wicklow Fire Station, recreational and sports facilities, and access to residential areas.

Within 1km of the Proposed Development, there are also numerous residential areas, other commercial properties such as a Lidl store south of the East Glendalough School, and other community facilities.

5 Proposed Development

5.1 Need and Objectives

The R750 Regional Road and Station Road suffer heavy congestion during peak hours caused by traffic coming to and from the East Glendalough School, WCC offices, and employment and residential areas. By improving high-quality pedestrian and cyclist infrastructure over the railway line and creating a link to the Wicklow Port Access Road, the Proposed Development aims to encourage a modal shift to walking and cycling for those accessing Station Road and Wicklow Train Station in order to partially alleviate peak-time congestion in the area.

The need for the Proposed Development is also recognised in the CDP through the following County Policy Objectives (CPO):

- **CPO 12.13:** *“To facilitate the development of pedestrian and cycle linkages through and between new and existing developments to improve permeability and provide shorter, more direct routes to schools, public transport, local services and amenities while ensuring that personal safety, particularly at nighttime, is of the utmost priority”;*
- **CPO 12.14:** *“To facilitate the implementation of local projects which support pedestrian and cyclist permeability, safety and access to schools and public transport”;*
- **CPO 12.19:** *“To support the development of car parks / set down areas to accommodate Park and Stride initiatives at appropriate locations, especially within walking distance to schools”;*
- **CPO 12.21:** *“To promote the development of transport interchanges and ‘nodes’ where a number of transport types can interchange with ease. In particular: [...] to enhance existing parking facilities at / near and the improvement of bus links to the train stations in Bray, Greystones, Kilcoole, Rathdrum, Wicklow and Arklow; [...] to improve existing and provide new footpath / footway linkages to existing / future transport interchange locations; [...]”;* and
- **CPO 12.23:** *“To ensure the continued and long-term operation of and improvement of the Dublin – Rosslare line, including the re-opening of closed stations, are maintained and to ensure that land uses adjacent to former stations are appropriate and can facilitate future improvements. [...]”;*

The main objectives of the Proposed Development are:

- To provide a high-quality pedestrian and cyclist link between the Wicklow Port Access Road, the Wicklow Train Station, and Station Road;
- To accommodate vehicles and buses setting down in the area accessed from the Wicklow Port Access Road; and
- To facilitate a modal shift to cycling, walking, and rail transport.

5.2 Components and Design

The Proposed Development extends over an area of approximately 1.34ha and includes the following:

- Construction of a car park and set-down facility that will include three bus parking spaces, a set-down area, 27 no. private vehicle car parking spaces (including three electric vehicle bays), three disabled car parking spaces and three age-friendly parking spaces;
- Construction of cycle tracks and footpaths;
- Construction of a pedestrian and cycle 33m long bridge spanning the Dublin to Rosslare Railway line to accommodate a 5.5m wide cycle track and footpath;
- Installation of three stair structures, one ramp structure which will accommodate an underground bike parking facility, and two lifts;
- Soft and hard landscaping, plus public lighting;
- All associated site and engineering works; and
- All associated accommodating works.

The Proposed Development layout is presented in Figure 5.1 and Appendix A of this report. The Proposed Development has been designed in accordance with relevant guidelines such as the NTA's 'Cycle Design Manual' (2023), and the Department of Transport's 'Traffic Management Guidelines' (2003) and 'Design Manual for Urban Roads and Streets' (DMURS) (2013).

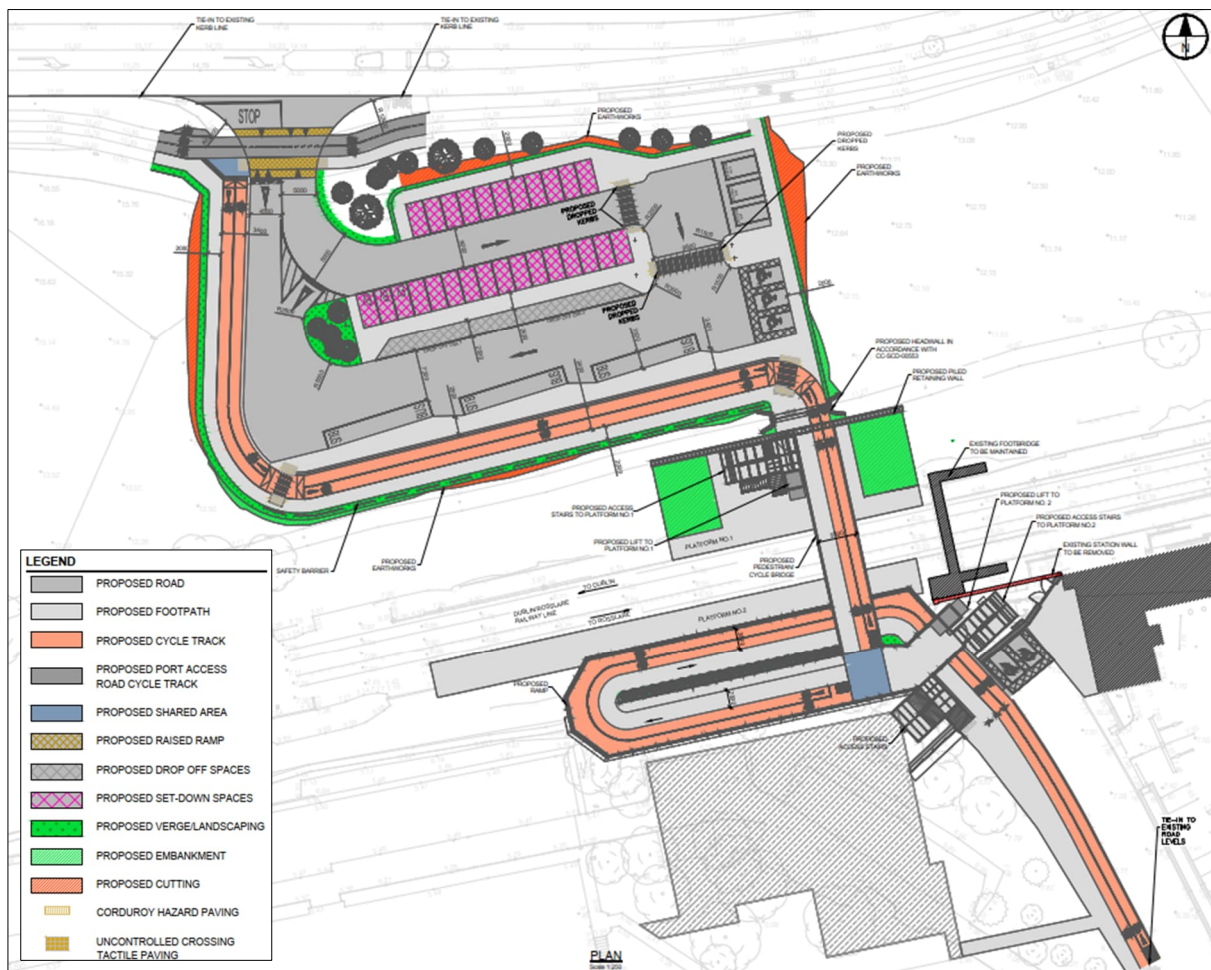


Figure 5.1: Proposed Development Layout

5.3 Drainage

Surface runoff will be collected via a network of gullies and subsurface carrier pipes which will convey flows into a geocellular attenuation tank. The attenuated runoff will then be discharged into the existing nearby drainage network on the Wicklow Port Access Road and Station Road, as agreed with the WCC Drainage Department. A petrol interceptor is proposed to capture any hydrocarbon spills within the car park and set-down area.

5.4 Utilities

A review of existing utilities within and in the vicinity of the Proposed Development site was undertaken in 2022. The following services were recorded in the area at the time:

- Eir;
- ESB Networks;
- Gas Networks Ireland (GNI);
- Virgin media; and
- Uisce Éireann (formerly Irish Water).

Changes to the layout of existing services will be required and new connections are proposed; the final layout of these services will be determined at detailed design stage when utility services are updated. Disruption of services (e.g., electricity outages) will not be required for the completion of the works.

It is anticipated that a water supply will be required during the construction phase.

5.5 Construction Phase

Construction phase activities include, amongst others, removal of vegetation and site clearance, breakthrough of existing surfaces and earthworks, backfilling and reinstatement of surfaces, construction of a bridge structure over the railway line and all other proposed structures, landscaping, connection to services, introduction of new road signage and markings, and all ancillary works. Subject to planning approval, construction of the Proposed Development is estimated to take two years to complete. Works over the railway line will require a rail possession for one weekend.

A key mechanism for managing potential noise and vibration impacts will be through adherence to site working hours as agreed with WCC. Site working hours are anticipated to be:

- 07:00 – 19:00 Monday to Friday;
- 08:00 – 14:00 Saturday; and
- No noisy works will take place on Sundays or bank holidays.

Where especially noisy works are to take place, taking into account relevant standards such as the British Standards Institution (BSI) *'Code of practice for noise and vibration control on construction and open sites – Part 1: Noise'* (BS 5228-1:2009+A1:2014) (2014), the appointed contractor (hereafter referred to as the 'Contractor') will contact WCC and residents who may be affected by noise and vibration to inform them of the intended location and the duration of the works.

The construction works will be undertaken in accordance with safeguards included in a **Construction Environmental Management Plan (CEMP)**. This will ensure that construction is undertaken in line with industry best practices. The CEMP will set out a range of measures to avoid and mitigate potential adverse environmental effects associated with the Proposed Development construction phase. It will include measures such as controls over the routing of construction vehicles, construction noise levels, drainage, and the handling and disposal of potentially contaminated soil and materials. The CEMP will also include measures/recommendations set out in other reports prepared for this planning application, such as the Architectural Heritage Assessment (AHA) prepared for the Proposed Development, as well as details of any environmental monitoring requirements, communication protocols, and particular measures as required by conditions associated with planning approval (if granted).

Measures included in the CEMP will be in accordance with all relevant standards and specifications, as well as best practice pollution prevention guidance and monitoring techniques. These include, for example:

- British Standards Institution (BSI) (2012), *'Trees in Relation to Design, Demolition and Construction'* (BS 5837:2012);
- Building Research Establishment (BRE) (2003), *'Control of Dust from Construction and Demolition Activities'*;
- Construction Industry Research and Information Association (CIRIA) (2001), *'Control of Water Pollution from Construction Sites - Guidance for Consultants and Contractors'* (C532);
- CIRIA (2006), *'Control of water pollution from linear construction projects. Site guide'* (C649);
- CIRIA (2006), *'Control of water pollution from linear construction projects. Technical guidance'* (C648D);
- CIRIA (2021), *'Archaeology and Construction: Good Practice Guidance'* (C799);
- CIRIA (2023), *'Environmental Good Practice on Site Guide (fifth edition)'* (C811);
- National Roads Authority (NRA) (2011), *'Guidelines for the Treatment of Air Quality during Planning and Construction of National Road Schemes'*;
- NRA (2014), *'Good Practice Guidance for the Treatment of Noise during the Planning of National Road Schemes'*; and
- Transport Infrastructure Ireland (TII) (2015), *'Drainage Systems for National Roads'* (DN-DNG-03022).

The Contractor will be responsible for preparing, implementing, and reviewing the CEMP throughout the construction phase of the Proposed Development.

In addition, should invasive species be identified onsite, an **Invasive Species Management Plan (ISMP)** will be prepared prior to the commencement of the construction works.

Waste and materials management during the construction phase will be managed in accordance with a **Resource and Waste Management Plan (RWMP)**. The RWMP will include consideration of opportunities to design out waste and improve materials efficiency, with efforts made to maximise on-site reuse and off-site recycling and recovery of any waste construction material generated. The Contractor will be responsible for preparing, implementing, and reviewing the RWMP through the construction phase, including the management of all suppliers and sub-contractors. The RWMP shall form a sub-plan of the Contractor's CEMP.

A **Construction Traffic Management Plan (CTMP)** will also be prepared as part and form a sub-plan of the Contractor's CEMP and agreed with WCC. The CTMP will adhere to relevant guidelines and requirements, such as the Department of Transport's *Traffic Signs Manual Chapter 8: Temporary Traffic Measures and Sign Roadworks* (2019) and Safety, Health & Welfare at Work legislation including the 2005 Act, the Safety, Health, and Welfare (Construction) Regulations 2013, and any amendment to them (the Construction Regulations).

5.6 Operational Phase

Upon the completion of the works, the Proposed Development will be operated as per the design to ensure high-quality pedestrian and cyclist link between the Wicklow Port Access Road, the Wicklow Train Station, and Station Road; to accommodate vehicles and buses setting down in the area accessed from the Wicklow Port Access Road; and to facilitate a modal shift to cycling, walking, and rail transport.

The design working life of the proposed bridge will be a minimum of 120 years as per TII guidelines 'Design for Durability DN-STR-03012' (2016), making the Proposed Development a permanent addition to local infrastructure. Maintainable elements and components that are subjected to greater wear will require replacement within the design life. Careful design and detailing, combined with thorough routine inspections, quality control, and supervision onsite, will help achieve the minimum expected design life of 50 years for these elements of the Proposed Development. Other maintenance works such as resurfacing of the car park and set-down area and repainting of road markings will be carried out as well, as necessary.

Inspections and maintenance of the proposed bridge will be carried out in accordance with TII's 'EIRSPAN Bridge Management System Routine Maintenance Manual' (AM-STR-06055) (2022) and 'EIRSPAN Bridge Management System Principal Inspection Manual' (AM-STR-06054) (2022). Routine inspections will be carried out at least once a year, while the interval between two principal inspections will be from one to six years, depending on the condition of the structure and the level of traffic experienced. The inspection will include a check of all visible structural components and, if high levels of deterioration are identified, the inspection interval will be decreased.

At the end of the estimated design life, the Applicant will make a decision to retain or decommission the bridge. For this reason, the decommissioning phase is not included as part of this EIA Screening. In addition, maintenance works during the operational phase, as well as potential effects associated with them, will be similar to those of the construction phase but smaller in scale, and have therefore not been considered separately.

5.7 Decommissioning Phase

The decommissioning phase is not considered as part of this EIA Screening Report as the Proposed Development is envisioned to become a permanent addition to the local rail network infrastructure.

6 EIA Screening

It is necessary to determine whether the Proposed Development constitutes EIA development under the Planning and Development Regulations (2001) (as amended) and the Roads Act 1993 (as amended).

6.1 Roads Act 1993 (as amended), Section 50

As the Proposed Development is not a type of development listed within Table 6.1, an EIA culminating in the preparation of an EIAR is not required as per the requirements of the Roads Act 1993 (as amended).

Table 6.1: Screening Criteria under the Roads Act 1993 (as amended)

Criteria	Comment	Is EIA Required on this Basis?
<i>S. 50.- (1) (a) A road development that is proposed that comprises any of the following shall be subject to an environmental impact assessment:</i>		
<i>(i) Construction of a motorway</i>	The Proposed Development does not include the construction of a motorway.	No
<i>(ii) Construction of a busway</i>	The Proposed Development does not include the construction of a busway.	No
<i>(iii) Construction of a service area</i>	The Proposed Development does not include the construction of a service area.	No
<i>The prescribed types are given in Article 8 of the Roads Regulations, 1994 (Road development prescribed for the purposes of S. 50(1)(a)(iv) of the Roads Act 1993 (as amended) as:</i>		
<i>a) The construction of a new road of four or more lanes, or the realignment or widening of an existing road so as to provide four or more lanes, where such new, realigned or widened road would be eight kilometres or more in length in a rural area, or 500 metres or more in length in an urban area</i>	The Proposed Development does not include the construction of a new road of four or more lanes, or the realignment or widening of an existing road so as to provide four or more lanes.	No
<i>(b) The construction of a new bridge or tunnel which would be 100 metres or more in length.</i>	The Proposed Development includes the construction of a bridge with a length of 33m.	No
<i>S. 50. – (1) (b) to (d) require that any road development or road improvement project which would be likely to have significant effects on the environment, including projects located on ecologically protected sites, shall be subject to EIA.</i>	The potential for likely significant effects has been considered further under Section 6.4 of this report.	No Refer to Section 6.4 of this report.

6.2 Planning and Development Regulations 2001 (as amended)

The following elements should be considered in determining whether the Proposed Development constitutes EIA development under the Planning and Development Regulations 2001 (as amended):

- If the proposed development is of a type listed in Schedule 5, Part 1;
- If not, whether:
 - It is listed in Schedule 5, Part 2; and
 - Any part of it is located within a sensitive area; or
 - It meets any of the relevant thresholds and criteria set out in Schedule 5, Part 2; and/or
 - It would be likely to have significant effects on the environment.

6.2.1 Schedule 5 Part 1

EIA is mandatory for developments listed in Schedule 5, Part 1 of the Planning and Development Regulations 2001 (as amended). Schedule 5, Part 1 developments are large scale developments for which significant environmental effects would be expected and comprise developments such as new airports and power stations.

The Proposed Development is not a type listed in Schedule 5, Part 1.

6.2.2 Schedule 5 Part 2

Part 2 of Schedule 5 of the Planning and Development Regulations 2001 (as amended) sets out specified limits for proposed developments for which an EIA culminating in the preparation of an EIAR is required, should a proposed development exceed the specified limits.

The screening of the Proposed Development against Part 2 of the Planning and Development Regulations 2001 (as amended) is contained in Table 6.2.

Table 6.2: Screening against relevant thresholds under Section 5, Part 2

Criteria	Regulatory Reference	Comment	Is EIA Required on this Basis?
<i>Urban development which would involve an area greater than 2 hectares in the case of a business district, 10 hectares in the case of other parts of a built-up area and 20 hectares elsewhere. (In this paragraph, "business district" means a district within a city or town in which the predominant land use is retail or commercial use.)</i>	Schedule 5, Part 2, 10 (b)(iv) of the Planning and Development Regulations 2001 (as amended).	The Proposed Development has an area of 1.34ha and is located in a built-up area.	No
<i>Any change or extension of development already authorised, executed or in the process of being executed (not being a change or extension referred to in Part 1) which would: (i) result in the development being of a class listed in Part 1 or paragraphs 1 to 12 of Part 2 of this Schedule, and (ii) result in an increase in size greater than: 25 per cent, or an amount equal to 50 per cent of the appropriate threshold, whichever is the greater.</i>	Schedule 5, Part 2, 13(a) of the Planning and Development Regulations 2001 (as amended).	The Proposed Development is an extension of an already executed development (i.e., the Wicklow Train Station) but it would not result in it being of a class listed in Part 1 or paragraphs 1 to 12 of Part 2 of this Schedule.	No
<i>Any project listed in this Part which does not exceed a quantity, area or other limit specified in this Part in respect of the relevant class of development, but which would be likely to have significant effects on the environment, having regard to the criteria set out in Schedule 7</i>	Schedule 5, Part 2, 15 of the Planning and Development Regulations 2001 (as amended).	The potential for likely significant environmental effects has been considered in Section 6.4 of this report.	No Refer to Section 6.4 of this report.

Source: Planning and Development Regulations 2001 (as amended)

As the Proposed Development is not a type of development identified in Schedule 5 Part 1 or Part 2 of the Planning and Development Regulations 2001 (as amended), there is no automatic requirement under the EIA Directive for it to be subjected to EIA. However, this EIA Screening Report has been prepared to establish whether there are likely significant environmental effects from the Proposed Development on the receiving environment with regard to Schedule 7A and Schedule 7 of the Planning and Development Regulations 2001 (as amended).

6.3 Selection Criteria for Screening Schedule 5 Development

Schedule 7 of the Planning and Development Regulations 2001 (as amended) sets out the selection criteria for screening Schedule 5 developments. These relate to specific matters, including:

1. The characteristics of the development (discussed under Section 6.4.1 of this report);
2. The location of the development (discussed under Section 6.4.2 of this report); and
3. The characteristics of the potential impact (discussed under Section 6.4.3 of this report).

These factors need to be taken into account as part of the screening process and are set out below.

6.3.1 Characteristics of Proposed Development

According to Schedule 7 of the Planning and Development Regulations 2001 (as amended), *“the characteristics of developments must be considered, with particular regard to:*

- a) *the size and design of the whole development;*
- b) *cumulation with other existing development and/or approved development;*
- c) *the nature of any associated demolition works;*
- d) *the use of natural resources, in particular land, soil, water and biodiversity;*
- e) *the production of waste;*
- f) *pollution and nuisances;*
- g) *the risk of major accidents and/or disasters relevant to the development concerned, including those caused by climate change, in accordance with scientific knowledge; and*
- h) *the risks to human health.”*

6.3.2 Location of Proposed Development

According to Schedule 7 of the Planning and Development Regulations 2001 (as amended), *“the environmental sensitivity of geographical areas likely to be affected by developments must be considered, with particular regard to:*

- a) *the existing and approved land use;*
- b) *the relative abundance, availability, quality and regenerative capacity of natural resources (including soil, land, water and biodiversity) in the area and its underground;*
- c) *the absorption capacity of the natural environment, paying particular attention to the following areas:*
 - i. *wetlands, riparian areas, river mouths;*
 - ii. *coastal zones and the marine environment;*
 - iii. *mountain and forest areas;*
 - iv. *nature reserves and parks;*
 - v. *areas classified or protected under legislation, including Natura 2000 areas designated pursuant to the Habitats Directive and the Birds Directive and;*
 - vi. *areas in which there has already been a failure to meet the environmental quality standards laid down in legislation of the European Union and relevant to the development, or in which it is considered that there is such a failure;*
 - vii. *densely populated areas; and*

viii. *landscapes and sites of historical, cultural or archaeological significance.*”

6.3.3 Types and Characteristics of Potential Impacts

According to Schedule 7 of the Planning and Development Regulations 2001 (as amended), “*the likely significant effects on the environment of a proposed development in relation to criteria set out under paragraphs 1 and 2, with regard to the impact of the development on the factors specified in paragraph (b)(i)(I) to (V) of the definition of ‘environmental impact assessment report’ in section 171A of the Planning and Development Act 2000 (as amended), take into account:*

- a) *the magnitude and spatial extent of the impact (for example, geographical area and size of the population likely to be affected);*
- b) *the nature of the impact;*
- c) *the transboundary nature of the impact;*
- d) *the intensity and complexity of the impact;*
- e) *the probability of the impact;*
- f) *the expected onset, duration, frequency and reversibility of the impact;*
- g) *the cumulation of the impact with the impact of other existing and/or development the subject of a consent for proposed development for the purposes of section 172(1A)(b) of the Act and/or development the subject of any development consent for the purposes of the Environmental Impact Assessment Directive by or under any other enactment, and*
- h) *the possibility of effectively reducing the impact.”*

The following section sets out a review of the above criteria and requirements specifically addressing the Proposed Development.

6.4 Schedule 7 Criteria Table

6.4.1 Characteristics of the Proposed Development

Table 6.3: Characteristics of the Proposed Development

Criteria	Proposed Development
(a) the size and design of the whole of the proposed development	
<i>Will the size and design of the whole project be considered significant?</i>	<p>The Proposed Development site has an area of 1.34ha. It involves the construction of a new pedestrian and cyclist bridge to provide a link between the Wicklow Port Access Road and Station Road, a car park and set-down area, and other ancillary elements and works such as access stairs, lifts, footpaths, and landscaping. The tallest structure (associated with the lifts to the train station platforms) will be approximately 8.8m high. The Proposed Development has been designed in line with relevant standards, guidelines, and recommendations, and scaled to fit into the existing Wicklow Train Station and the urban setting while taking into consideration the needs of pedestrians, cyclists, and persons with disabilities.</p> <p>The works and changes proposed are considered not significant in relation to their size and design within the context of the surrounding environs, and will enhance convenience and accessibility for local residents.</p>
(b) cumulation with other existing development	
<i>Will other existing project and/ or approved project be able to affect the project?</i>	<p>A desktop search of proposed and existing planning applications was carried out on 9 September 2024. The search used publicly available data from the MyPlan.ie’s ‘National Planning Application’ database, An Bord Pleanála’s (ABP) database, and WCC’s Planning Portals. The scope of the search was based within a 3 kilometres (km) radius from the approximate centrepiece of the Proposed Development and limited to committed developments which have been approved by WCC or ABP within the last five years or are currently pending a planning decision.</p> <p>The majority of developments identified are of small scale in nature (e.g., developments such as single residential properties and retention projects) or are considered to be a sufficient distance from the Proposed Development site so as not to warrant further consideration. Only reasonably foreseeable developments</p>

Criteria

Proposed Development

were considered. The identified relevant planning applications are listed in Appendix B. These mostly comprise residential developments, in addition to some small-scale commercial developments or community services/facilities, for example:

- **WCC Ref. no. 24252:** located at a site approximately 190m south of the Proposed Development, this application is for the expansion (380 square metres (m²) gross internal floor area) of recreational/sports facilities at the Coral Leisure Centre. This Part 8 application was approved in September 2024.
- **ABP Ref. no. 318978:** for the demolition of existing structures and construction of 13 no. houses, in addition to a new internal road and footpaths, landscaping, public lighting, and all other associated works, at a site located approximately 880m south-west of the Proposed Development. This application is currently pending a planning decision.
- **ABP Ref. no. 312888:** for the construction of 66 no. dwellings and all associated works at a greenfield site of approximately 3.6ha located approximately 1.2km south-west of the site. This application was granted planning permission in May 2023 and construction works are currently underway.
- **ABP Ref. no. 312889:** for the construction of 84 no. dwellings and all associated works at a greenfield site of approximately 4.3ha located approximately 1.2km south-west of the site. This application was granted planning permission in May 2023 and construction works are currently underway.

A number of these developments have finished construction and are currently operational, while others have not commenced construction at the time of writing this report. It is therefore possible that the construction phase of the Proposed Development will, at least in part, overlap with that of other applications listed in Appendix B. In this case, there is potential for short-term cumulative effects with a number of these applications, such as effects on the local road network due to increased traffic and effects on water resources associated with runoff of sediments or pollutants from construction sites/works. However, these developments have gone through the planning process and will most likely, like the Proposed Development, implement standard and best practice mitigation measures to manage potential impacts. In addition, as the majority of permitted developments within 1km of the site have finished construction, certain cumulative effects (e.g., noise emissions from multiple construction sites in proximity to each other) are unlikely to occur. Should the construction phase of WCC Ref. no. 24252 overlap with that of the Proposed Development, this would be the only development likely to result in cumulative noise, traffic, and landscape and visual effects. However, providing compliance with standard best practice mitigation measures on all sites, **no significant cumulative effects** are likely to occur during the construction phase of the Proposed Development.

Once the construction phase of permitted residential developments in the surrounding environs is finalised, these developments will lead to an increase in the number of local residents in the area which is then likely to result in an increase in traffic and congestion in the area. By providing a new pedestrian and cyclist link between lands north and south of the Wicklow Station and infrastructure that allows for the expansion of public transport services, the Proposed Development will be able to support the sustainable development of the town and avoid or reduce the potential for increased demands on the local road network associated with other permitted developments.

The Proposed Development and other permitted applications will also result in an increase in hardstanding surfaces, which will increase surface runoff and demands on the capacity of the existing drainage network. Should applications propose inadequate drainage systems, or should the combined demand of all applications surpass the capacity of the local drainage network, this could result in an increased risk of flooding in already susceptible areas in the surrounding environs of the Proposed Development. However, WCC would have reviewed and approved the proposed drainage design for each site (and, where applicable, Sustainable Drainage Systems (SuDS)), or set out conditions to improve the proposed drainage systems, in order for all applications (including the Proposed Development) to be granted planning permission. Therefore, **no significant cumulative effects** associated with increased risk of flooding are likely to occur.

(c) the nature of any associated demolition works

Will the construction of the project include any significant demolition works? The Proposed Development does not involve major demolition works; only minor elements within the site boundary (e.g., a boundary wall, trees, scrub, and fences) will be removed.

(d) the use of natural resources, in particular land, soil, water and biodiversity

Will construction or operation of the project use natural resources above or below ground which are non-renewable or in short supply? The use of materials will be higher during the construction phase and limited to maintenance works, as needed, during the operational phase. These will likely include materials such as concrete, steel, asphalt, paint, Structural Fill material (6N), Capping Layer (6F), and Subbase (Type 1). Where materials are required, it has been assumed that, where practicable, they would be sourced from the local area to reduce transportation distances and brought to the work areas using the existing

Criteria	Proposed Development
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public infrastructure where possible. Exact quantities of materials required will be identified at the detailed design stage, however, these are unlikely to be significant given the scale and nature of the works.

It is anticipated that the Proposed Development will require cut of approximately 2,820.7m³ and fill of 92.3m³ during the construction phase. In addition, in order to accommodate the works, the Proposed Development will require the loss of approximately 5,500m² (0.55ha) of green space (comprised of approximately 1,600m² within the Glendalough Secondary School and 3,900m² of greenfield land) and a total of 212m of treelines/scrub (approximately 65m along the northern boundary of the site, 76m along the northern section of the railway line, and 71m along the northern boundary of the East Glendalough Secondary School). This includes an estimated 13no. trees to be removed, the majority of which are located within the Glendalough Secondary School.

A total of 1.34ha of permanent land acquisition are required for the completion of the Proposed Development, including a green areas along the eastern and northern boundaries of the East Glendalough Secondary School. The Applicant is engaged in land acquisition discussions with the relevant landowners who have expressed their support for the Proposed Development.

A water supply will be required during the construction phase. All relevant permissions will be sought prior to works commencing and at no point should water be abstracted or discharged to/from surface or ground waterbodies. A water supply is not required for the operational phase.

Given the nature of the Proposed Development, significant quantities of natural resources are unlikely to be required the construction and operational phases, and therefore **no significant effects** on natural resources are likely as a result of the Proposed Development.

(e) the production of waste

Will the project produce wastes during construction or operation or decommissioning?

The majority of the waste will be generated during the construction phase and will potentially comprise (but not be limited to) excavated soils, materials from hardstanding surfaces, vegetation, and generic construction waste. Exact quantities of construction waste will be identified at the detailed design stage; however, it is anticipated that the Proposed Development will require cut of approximately 2,820.7m³ of soil.

During the operational phase, waste generated will be similar to those of the construction phase during maintenance works, though lower in quantity, in addition to municipal waste generated during the normal operation of the facilities.

Where waste is produced, it will be managed in accordance with the RWMP and all relevant Irish waste management legislation and guidance and, in particular, any materials that cannot be reused (e.g., contaminated soils identified onsite) will only be transported by hauliers holding a valid collection permit to waste management sites which hold the necessary license, permit, certification, or exemption.

Given the scale, location, and type of Proposed Development, **no significant effects** on the capacity of on waste management facilities as a result of waste generation are likely to occur.

(f) pollution and nuisances

Will the project release any pollutants or any hazardous, toxic or noxious substances to air?

Dust and greenhouse gas (GHG) emissions from construction machinery and works will be short-term, reversible upon completion of works, and likely to be minor given the type and scale of the works. In addition, these emissions will be managed through the Contractor's CEMP which will include measures to avoid or reduce them; for example, plant and equipment will be maintained and turned off when not in use.

During the operational phase, given the nature of the Proposed Development, there will be no release of polluting or hazardous substances into the air.

Will the project cause:

Noise and vibration

Construction noise has the potential to result in adverse effects on nearby sensitive receptors such as receptors at the East Glendalough Secondary School and nearby residential receptors. However, these activities will be short-term in duration and programmed to reduce potential noise impacts on these receptors.

Due to the nature of the Proposed Development, **no significant** increase in noise emissions is likely to occur during the operational phase.

Release of light

Public lighting at the car park and set-down areas, the ramp and bridge, the platform, and paths leading to/from the site is proposed for the safe operation of the facilities; the lighting design is presented in Appendix A of this report. Given the location of the Proposed Development within an urban setting and the scale of the works proposed, **no significant effects** on the local population or sensitive ecological receptors are likely as a result of additional lighting introduced by the Proposed Development.

Heat

The Proposed Development will not cause release of heat.

Criteria	Proposed Development
Energy	The Proposed Development will not cause release of energy.
Electromagnetic radiation	The Proposed Development will not cause release of electromagnetic radiation.
<i>Will the project lead to risks of contamination of land or water from releases of pollutants, including leachate, onto the ground or into surface waters, groundwater, coastal waters or sea?</i>	<p>Leaks and spills of materials which contain hydrocarbons, or runoff of materials stored or managed incorrectly, have the potential to result in the contamination of nearby waterbodies. The Contractor's CEMP will include an emergency response procedure for any leaks and spills that may occur during the construction phase, as well as best practice measures to avoid or manage the risk of pollutants entering exposed soils and nearby water receptors. These will include, but are not limited to, guidelines listed in Section 5.5 of this report, and mitigation measures set out in Table 6.5 Criteria (h).</p> <p>Taking the above into consideration as well as the nature of the work, the risk of significant pollution incidents during the construction phase is considered low.</p> <p>During the operational phase, surface runoff from hardstanding surfaces such as the car park and set-down area, will be managed through the proposed drainage so that not significant effects are likely to occur.</p>
<i>Will the Proposed Development lead to nuisances to the population?</i>	<p>There is potential for increased traffic during the construction phase, which would be short-term and reversible upon the completion of the works. Potential impacts can be managed through the Contractor's CEMP and CTMP. Disruption of train services will be required for one weekend only and therefore no significant nuisances to the population are likely to occur.</p>
(g) the risk of major accidents, and/or disasters which are relevant to the project concerned, including those caused by climate change, in accordance with scientific knowledge	
<i>Will there be any risk of major accidents (including those caused by climate change, in accordance with scientific knowledge) during construction, operation or decommissioning?</i>	<p>Ireland in general is at low risk of natural disasters: earthquakes are rare and of low magnitude, there are no active volcanos, and severe weather events are rarely experienced. Flooding, however, is experienced throughout Ireland on a regular basis.</p> <p>The Proposed Development is not located within areas susceptible to experience fluvial, pluvial, or groundwater flooding, either at present or in future scenarios, or areas which have experienced past flood events; the closest such areas correspond to Catchment Flood Risk Assessment Management (CFAM) River and Coastal Flood Extents located approximately 38m south which extend along the GLEBE 10 (Segment Code: 10_1605) and the Broad Lough Estuary (OPW, 2024; GSI, 2024a). These areas prone to flooding are at a lower elevation compared to the Proposed Development site. Therefore, floods are unlikely to cause a significant environmental problem for the Proposed Development. In addition, as surface runoff will be managed through the proposed drainage design, no significant increase in the risk of flooding in nearby lands is likely to occur as a result of the Proposed Development.</p> <p>In terms of man-made disasters, the risk of accidents occurring during the construction phase will be avoided or managed through the implementation of the Contractor's CEMP (including emergency response procedures for any leaks and spills) and CTMP. Train services will be disrupted for one weekend to allow for the safe completion of works over the railway line. In addition, the Proposed Development has been designed in line with the relevant standards and guidelines to avoid or lower the risk of accidents during the operational phase (e.g., pedestrians falling from the bridge onto the railway line). Taking this into consideration, the risk of major accidents or disasters is considered low.</p>
<i>Is the location susceptible to earthquakes, subsidence, landslides, erosion, or extreme /adverse climatic conditions, e.g. temperature inversions, fogs, severe winds, which could cause the project to present environmental problems?</i>	<p>There are no karst features in the greater context of the Proposed Development site, and this area is not susceptible to earthquakes.</p> <p>The majority of the site is located within an area of 'Low' landslide susceptibility, with the exception of a limited area on the southernmost section of the site along Station Road which has 'Moderately Low' susceptibility. Ground Investigations (GI) have informed the design of the Proposed Development to ensure the safety and suitability of the facilities.</p> <p>There is only one record of a major storm (i.e., Storm Emma, from 28 February to 4 March 2018) in the area of the site for the last 10 years, which caused a significant spell of snow and low temperatures.</p>
(h) the risks to human health (for example, due to water contamination or air pollution)	
<i>Will the project present a risk to the population (having regard to population density) and their human health during construction, operation or decommissioning? (for example, due to water contamination or air pollution)</i>	<p>Construction projects contain an element of human risk. During the construction phase, there will be potential for impacts relating to dust and noise generation from construction activities as well as onsite accidents.</p> <p>According to the 2022 Census, approximately 87% of the population in the town of Wicklow consider themselves to be of 'Very Good' or 'Good' health, while approximately 2% consider themselves to be in 'Bad' or 'Very Bad' health. Additionally, Air Quality Index for Health (AQIH) for the general area of the Proposed Development is '3-Good'. Taking this into consideration, as well as the nature of the works, the short-term duration of the construction phase, and the implementation of</p>

Criteria	Proposed Development
	<p>appropriate mitigation measures during the construction phase, no likely significant effects on human health are likely to occur.</p> <p>No significant effects on human health are likely to occur during the Proposed Development operational phase. In addition, the Proposed Development aims to encourage a modal shift from private vehicles to active travel or public transport. This has the potential to reduce GHG emissions and increase physical activity, which subsequently has the potential to result in positive effects in human health.</p>

In summary, it is considered that the scale and characteristics of the Proposed Development indicate it would not constitute EIA development. Given the scale and design of the Proposed Development in the context of the surrounding land uses, the limited likely use of natural resources, and the likely low volume of waste generated, as well as the preparation and adoption of a CEMP, CTMP, and RWMP to mitigate the effects of construction works, the Proposed Development is unlikely to give rise to significant environmental effects.

6.4.2 Location of the Proposed Development

Table 6.4: Location of the Proposed Development

Criteria	Commentary
(a) the existing and approved land use	
<p><i>Are there existing or approved land uses or community facilities on or around the location which could be affected by the project?</i></p>	<p>The Proposed Development site extends over the existing Wicklow Train Station, a limited section of the East Glendalough Secondary School, and a greenfield site to the north of the railway line. Lands south of the railway line are zoned for 'Community/Educational/Institutional', while lands to the north are zoned for 'Enterprise and Employment'. However, the use of lands adjacent to train stations for their improvement is in line with the vision and objectives of the CDP, and the location of the Proposed Development will also support other CPOs which aim to improve accessibility to schools and public transport, particularly via active travel.</p> <p>Land use zoning in proximity to the site includes other 'Community/Educational/Institutional' and 'Enterprise and Employment', 'Passive Open Space', 'Existing Residential', 'New Residential', 'Mixed Use', and 'Town Centre'. There is also a riverwalk from the car park of the Wicklow Train Station towards Major's Cove on the east across Broad Lough Estuary.</p> <p>During the construction phase, access to the Wicklow Train Station and the East Glendalough Secondary School, the riverwalk, and all other lands in proximity to the site will be maintained. The Proposed Development will require 1.34ha of permanent land acquisition; however, as previously stated, relevant landowners who have expressed their support for the Proposed Development.</p> <p>Train services will continue to operate throughout the construction phase of the Proposed Development, with the exception of one weekend during which works over the railway line will be completed.</p> <p>During the operational phase, the Proposed Development will provide a new link between lands south and north of the railway line, reducing time spent commuting for local residents, and will improve accessibility conditions for persons with disabilities. It will also allow bus services to be expanded by introducing new set-down areas.</p> <p>Taking the above into consideration, no significant effects on existing and approved land uses are likely to occur as a result of the Proposed Development.</p>
(b) the relative abundance, availability, quality and regenerative capacity of natural resources (including soil, land, water and biodiversity) in the area and its underground	
<p><i>Are there any areas on or around the location which contain important, high quality or scarce resources which could be affected by the project?</i></p>	<p>Lands north of the railway line comprise a greenfield area with treelines and scrub that act as boundaries along the Wicklow Port Access Road and the railway line. There is also an area of amenity grassland with trees (including mature trees) associated with the East Glendalough Secondary School. The Proposed Development will require the permanent loss of approximately 5,500m² of green space (including 3,900m² of greenfield land) and a total of 212m of treelines/scrub, including 13no. trees. Taking into consideration the size of the works, the land use zoning designation, and the implementation of the Contractor's CEMP, no significant impacts on biodiversity due to habitat loss during the construction phase are likely to occur. There is also potential for damage to trees/scrub in proximity to the works; during the operational phase, any proposed planting that is diseased or fails to grow within 2 years of planting, or any existing vegetation that becomes damaged or diseased as a result of construction works, will be replaced.</p> <p>The site entirely located within an area classified as having 'High Vulnerability' to groundwater contamination and 'High' subsoil permeability, and it extends over the Wicklow ground waterbody (ID: IE_EA_G_076) which is described as 'Poorly productive bedrock' and has a 'Good' Overall Groundwater Status and is 'At Risk' of not meeting Water Framework Directive (WFD) objectives.</p>

Criteria	Commentary
	<p>No groundwater abstraction is required for the completion of the Proposed Development.</p> <p>It is anticipated that the Proposed Development will require cut of approximately 2,820.7m³ of soils. Soils within the site are identified as 'Basic Shallow Well Drained Mineral', while subsoils are classified as 'Limestone sands and gravels (Carboniferous)' north of the railway line and 'Man made' to the south.</p> <p>Taking into consideration the size and nature of the Proposed Development, and the characteristics of natural resources onsite as well as the implementation of the Contractor's CEMP, no significant effects on natural resources are likely to occur.</p>
(c) the absorption capacity of the natural environment, paying particular attention to the following areas:	
<i>(i) Are there any other areas on or around the location which has the potential to impact on the absorption capacity of the natural environment, paying particular attention to wetlands, riparian areas, river mouths?</i>	<p>The closest river waterbody is the Wicklow_010 (ID: IE_EA_10W080880), located approximately 43m south. This waterbody has a 'Good' Ecological Status or Potential and its WFD Risk is under review. It discharges into the Broad Lough Estuary; some areas in and around the estuary are identified as wetlands.</p> <p>No works on these waterbodies, or abstraction or discharge to/from them is required. During the construction phase, best practice measures included in the Contractor's CEMP (including an emergency spill response plan) will be implemented to avoid or reduce the risk of impacts to wetlands, riparian areas, and river mouths.</p> <p>Taking into consideration the nature of the works proposed, the implementation of the Contractor's CEMP, and that the proposed drainage will avoid or reduce the likelihood of untreated surface runoff from entering these waterbodies during the operational phase, no significant effects on these waterbodies are likely to occur as a result of the Proposed Development.</p>
<i>(ii) Has the project the potential to impact on the absorption capacity of the natural environment, paying particular attention to coastal zones and the marine environment?</i>	<p>Broad Lough (ID: IE_EA_130_0100), a transitional waterbody, is located approximately 34m east from its closest point of the site. It has a 'Good' Biological Status or Potential but 'Moderate' Chemistry Conditions, and is 'At Risk' of not meeting WFD objectives. This waterbody is hydrologically connected to the Southwestern Irish Sea - Killiney Bay (HA10) (ID: IE_EA_100_0000), located approximately 500m east. This coastal waterbody has a 'High' Ecological Status or Potential and is 'Not at Risk' of not meeting WFD objectives.</p> <p>With the implementation of the Contractor's CEMP and proposed drainage measures, and given the nature of the works proposed, no significant effects on coastal zones and the marine environment are likely to occur as a result of the Proposed Development.</p>
<i>(iii) Has the project the potential to impact on the absorption capacity of the natural environment, paying particular attention to mountain and forest areas?</i>	<p>The closest site recorded in the Ancient and Long-Established Woodland Inventory is Cronroe, approximately 4.3km northwest (NPWS, 2010). The closest mountain is Carrick Mountain approximately 7.5km west.</p> <p>Due to the distance to the site and the nature of the works proposed, no significant effects on mountains or forested areas are likely to occur.</p>
<i>(iv) Has the project the potential to impact on the absorption capacity of the natural environment, paying particular attention to nature reserves and parks?</i>	<p>The closest nature reserve or park is the Deputy's Pass Nature Reserve, located approximately 7.8km southwest. There is also an area of trees and woodlands with existing Tree Preservation Orders (TPO) approximately 230m southwest of the site.</p> <p>Due to the distance from the site and the nature of the works proposed, no significant effects on nature reserves and parks are likely to occur.</p>
<i>(v) Has the project the potential to impact on the absorption capacity of the natural environment, paying particular attention to areas classified or protected under legislation, including Natura 2000 areas designated pursuant to the Habitats Directive and the Birds Directive?</i>	<p>Broad Lough Estuary forms part of two international and one national designations: the Murrough Special Protection Area (SPA) (ID: 004186) (approximately 41m southeast), the Murrough Wetlands Special Area of Conservation (SAC) (ID 002249) (approximately 111m east), and the Murrough proposed Natural Heritage Area (pNHA) (ID: 000730) (approximately 118m east). There is a second pNHA in the greater surrounding environs of the Proposed Development, i.e., the Wicklow Town Sites pNHA (ID: 001929) approximately 307m east.</p> <p>The AA Screening prepared for the Proposed Development in September 2024 concludes that:</p> <p><i>"In view of best scientific knowledge and on the basis of objective information in light of the Conservation Objectives of the relevant European Sites, it can be stated beyond reasonable scientific doubt, that the Proposed Development will not have a significant effect on any European site, either individually or in-combination with other plans and projects.</i></p> <p><i>There is consequently no requirement to proceed to the next stage of Appropriate Assessment."</i></p>

Criteria	Commentary
<p>(vi) <i>Has the project the potential to impact on the absorption capacity of the natural environment, paying particular attention to areas in which there has already been a failure to meet the environmental quality standards laid down in legislation of the European Union and relevant to the project, or in which it is considered that there is such a failure?</i></p>	<p>Broad Lough and the Wicklow ground waterbody are 'At Risk' of not meeting WFD objectives. As previously stated, no significant effects on these waterbodies are likely as a result of the Proposed Development.</p>
<p>(vii) <i>Has the project the potential to impact on the absorption capacity of the natural environment, paying particular attention to densely populated areas?</i></p>	<p>The Proposed Development is located within the Key Town of Wicklow, in proximity to the northern boundary of this settlement. According to the Census 2022, This town has a population of 12,957 persons, which is a growth of approximately 22% compared to the Census 2016. During the construction phase, there is potential for nuisances associated with increased traffic and noise emissions from construction activities, particularly on sensitive receptors such as the East Glendalough School and nearby residential areas. However, these impacts would be short-term, reversible upon the completion of the works, and managed through the Contractor's CEMP. Interruptions to train services will be limited to one weekend. In addition, during the operational phase, the Proposed Development will provide a new link between lands south and north of the railway line that will improve convenience for locals commuting to and from these areas, including the school adjacent to the Proposed Development.</p> <p>No significant effects on densely populated areas are likely to occur as a result of the Proposed Development.</p>
<p>(viii) <i>Has the project the potential to impact on the absorption capacity of the natural environment, paying particular attention to landscapes and sites of historical, cultural or archaeological significance?</i></p>	<p>Landscape</p> <p>According to the CDP's Landscape Character Assessment (LCA), the site is within urban environments and there are no sensitive landscape designations in or in close proximity to the site. The closest of such designations are prospects of coast along railway line from Greystones to Wicklow town, located approximately 390m east at its closest location from the site, and a linear geological heritage site located approximately 396m east, described as "an uninterrupted shingle beach extending for over 17km long between Greystones and Wicklow".</p> <p>Visual impacts from the presence of construction works and machinery will be short-term. Upon the completion of construction works, the existing landscape character and appearance will remain similar to the existing conditions which is already defined by the Wicklow Train Station and the transport network. Aesthetic considerations have informed the design of the proposed bridge, including the selection of materials, finishes, and shape of the bridge elements, to ensure its presence during the operational phase does not result in a significant impacts on the landscape. In addition, views of the Proposed Development from the linear prospect along the coast are restricted by intervening elements in the landscape, including the Wicklow Port Access Road which contains an elevated bridge structure to allow for an underpass leading into the L11011 Local Road. The Proposed Development will therefore fit into the existing setting and is unlikely to result in significant landscape and visual effects as a result of the Proposed Development.</p> <p>Taking the above into consideration, no significant effects on the landscape and visual character of the area are likely to occur as a result of the Proposed Development.</p> <p>Cultural Heritage</p> <p>The area to the north of the main railway station building is within the curtilage of one asset listed in the Record of Protected Structures (RPS) (i.e., WTC-RPS 25) and includes a wall which will be removed under the Proposed Development. The AHA prepared for the Proposed Development notes that "while there will be no changes to the front elevation of the building, there will be a loss of space directly outside the building due to the access stairs and the new boundary wall which will replace the current boundary wall". Although the AHA identifies no significant effects as a result of the removal of the wall or other works associated with the Proposed Development, it sets out the following recommendations to avoid impacts on the setting of cultural heritage assets:</p> <ul style="list-style-type: none"> • The original boundary wall should be subject to recording prior to removal; • The render of the replacement wall must be appropriate to the setting of the WTC-RPS 25; and • The finish of the surfaces must be appropriate to the setting of the WTC-RPS 25.

Criteria	Commentary
	<p>According to the Heritage Maps and the CDP, there are no other cultural heritage assets within or in proximity to the site. The closest assets are:</p> <ul style="list-style-type: none"> • Brickfield Lane Architectural Conservation Area (ACA), located approximately 380m southeast; • A National Inventory of Architectural Heritage (NIAH) site (i.e., Greenmount house), located approximately 400m southeast within the Brickfield Lane ACA; and • A Sites & Monuments Record (SMR) and associated Zoned of Notification (ZoN), located approximately 280m northwest, which corresponds to an excavation site opened in 2006 which uncovered archaeological findings. <p>In addition, there is potential for unrecorded archaeological remains be present on the greenfield site north of the railway line which could be damaged during excavation works. However, large excavations are not anticipated for the completion of the works.</p> <p>Taking into consideration the implementation of recommendations listed in the AHA and the Contractor's CEMP, and given the type of Proposed Development and the distance between the site and other cultural heritage assets, no significant effects on cultural and archaeological heritage assets are likely to occur as a result of the construction or operational phases of the Proposed Development.</p>

It is considered that the location of the Proposed Development will not give rise to its definition as EIA development. The Proposed Development will be located in greenfield land zoned for 'Enterprise and Employment', the Wicklow Train Station, and a limited green area within the boundaries of the East Glendalough School. The proposed works will be in line with land use and policy objectives set out in the CDP for the area, and will improve connectivity and accessibility between lands north and south of the railway line for pedestrians, cyclists, and persons with disabilities. The Proposed Development is in proximity to areas of notable natural resources and protected sites (e.g., the Broad Lough Estuary and associated conversation sites), however the site itself and adjacent lands are located in an urban setting and contain limited natural resources in terms of soil, land, and water that could be affected by the proposed works. With suitable embedded and control measures in place (as relevant during the construction or operational phases), significant environmental effects are unlikely to occur as a result of the Proposed Development.

6.4.3 Types and Characteristics of Potential Impacts

Table 6.5: Types and Characteristics of Potential Impacts

Criteria	Commentary
(a) the magnitude and spatial extent of the impact (for example, geographical area and size of the population likely to be affected)	<p><i>Outline the magnitude and spatial extent of the impact (for example, geographical area and size of the population likely to be affected)</i></p> <p>The Proposed Development extends over an area of approximately 1.34ha. During the construction phase, direct adverse impacts are likely to extend over the site and surrounding environs in close proximity. Receptors likely to be impacted are persons using the Wicklow Train Station and Station Road to commute, receptors at the East Glendalough School, residential receptors (particularly those at Church Gate), and receptors at sports, civil service, and employment facilities along Station Road. During the disruption of train services, impacts are likely to extend to persons outside the town of Wicklow (e.g., tourists travelling by train to visit the town from other settlements along the line, or workers with weekend shifts).</p> <p>In addition, without the implementation of mitigation measures to be included in the Contractor's CEMP (e.g., emergency response procedures in the event of accidental spills), there is potential for impacts to extend onto nearby water receptors and conservation areas, as well as other sites hydrologically linked to them, with indirect effects on ecological features associated with these sites.</p> <p>During the operational phase, the Proposed Development will have a positive effect on the larger area by improving connectivity between lands north and south of the railway line. Adverse effects from landscape changes as a result of the introduction of the proposed bridge are likely to extend to areas beyond the Proposed Development and adjacent lands, depending on changes in local topography and the presence of intervening elements in the landscape.</p>

Criteria

Commentary

(b) the nature of the impact

Outline the nature of the impact

In the absence of mitigation measures, potential adverse impacts associated with the construction phase include:

- Landscape and visual impacts from the presence of construction works/machinery;
- Impacts on local air quality associated with GHG and dust emissions from construction vehicles, equipment, and works;
- Increased noise levels from construction works and equipment;
- Increased traffic and transport delays, potential for transport-related accidents, and disruption to train services;
- Potential risks to surface and ground waterbodies associated with accidental spills and leaks, sediment loading, and runoff of contaminants;
- Risks to human health and nuisances as a result of the aforementioned impacts, particularly on residents in close proximity to the site, such as those in residential areas, users of the Wicklow Train Station, and the East Glendalough School;
- Loss of approximately 5,500m² of green space (including 3,900m² of greenfield land) and a total of 212m of treelines/scrub (including 13no. trees), and potential for damage to trees/scrub in proximity to the works;
- Disturbance and/or accidental injury or entrapment of local fauna (e.g., overnight entrapment of small mammals or disturbance to nesting birds during site clearance), and loss of suitable habitat;
- Potential risk of spread of invasive species, should they be identified on site;
- Impacts on lands and relevant landowners from permanent land acquisition requirements;
- Impacts on soils from cut requirements and potential for contamination of exposed soils by way of accidental spills and/or surface runoff;
- Indirect impacts on conservation sites and animal/plant species as a consequence of impacts on water resources;
- Water supply demands during the construction phase;
- Potential for permanent impacts on previously unrecorded archaeological remains as a result of excavations on greenfield land, should they exist;
- Direct and indirect impacts on WTC-RPS 25 as a result of the removal of a wall and visual impacts on its setting; and
- Impacts on the capacity of waste management facilities to handle construction waste.

Potential adverse impacts associated with the operational phase include:

- Impacts on local fauna and residents as a result of light disturbance from the Proposed Development;
- Landscape and visual impacts as a result of changes introduced by the Proposed Development;
- Indirect impacts on cultural heritage assets in proximity to the site by way of visual impacts on their setting;
- Risk of accidents occurring onsite (e.g., pedestrian fall from the bridge);
- Impacts on the capacity of waste management facilities to handle operational waste;
- Impacts on water receptors due to runoff of surface contamination associated with vehicle use of roads (e.g., drips and spills of hydrocarbons); and
- Impacts on the capacity of the drainage network due to increased hardstanding surfaces.

Potential positive impacts associated with the operational phase include:

- Increased accessibility between lands north and south of the railway line, including improved access to schools and public transport, for pedestrians, cyclists, and persons with disabilities;
- Increase availability of set-down areas which will support the improvement of bus links;
- Modal shift from private vehicles to active travel and public transport;
- Reduced congestion and noise emissions along Station Road as a result of a reduction in the use of private vehicles;
- Improved local air quality and human health as a result of a reduction in GHG emissions from private vehicles and an increase in active travel;
- Improved convenience for the local population as a result of reduced congestion and time spent commuting; and
- Positive impacts on local material assets through the provision of a new bridge crossing.

Criteria	Commentary
(c) the transboundary nature of the impact	
<i>Is the project likely to lead to transboundary effects?</i>	Given the type, size, and scale of the Proposed Development, as well as its distance to Northern Ireland (approximately 122km to the north by land) and Wales (approximately 88km east by sea), no significant transboundary impacts are likely to occur as a result of the Proposed Development.
(d) the intensity and complexity of the impact	
<i>Outline the intensity and complexity of the impact</i>	No significant impacts are anticipated to occur as a result of the construction or operational phases of the Proposed Development. In addition, some impacts can be avoided or reduced through mitigation measures embedded into the design of the Proposed Development (e.g., proposed drainage system intercepting potential road surface contamination) or implemented through the Contractor's CEMP.
(e) the probability of the impact	
<i>Outline the probability of the impact</i>	<p>Significant adverse environmental impacts on the receiving environment resulting from the Proposed Development are unlikely to occur given the type, location, size, and scale of the Proposed Development, as well as embedded mitigation measures, the implementation of the Contractor's CEMP, CTMP, and inherent controls, and adherence to appropriate national guidelines and codes of practice.</p> <p>During the operational phase, the Proposed Development is likely to result in a positive impact on the local road network and traffic conditions as well as convenience for the community by providing a new pedestrian and cyclist link which will provide an alternative commuting route and support the improvement of bus links.</p>
(f) the expected onset, duration, frequency and reversibility of the impact	
<i>Outline the expected onset, duration, frequency and reversibility of the impact</i>	<p>With the appropriate mitigation measures, the majority of the impacts associated with the construction phase will be short-term and reversible upon the completion of the works. Impacts associated with the disruption of train services will be limited to a weekend. Loss of green spaces and trees/scrub, and land acquisition requirements will be permanent. Impacts on commuters passing in proximity to construction works would be transient.</p> <p>During the operational phase, the Proposed Development will result in long-term positive impacts on the road network and traffic conditions as well as the local community.</p>
(g) the cumulation of the impact with the impact of other existing and/or development	
<i>Could this project together with existing and/ or approved project result in cumulation of impacts together during construction/ operation phase?</i>	<p>A list of the cumulative developments considered is included in Appendix B of this report. During the construction phase, there is potential for cumulative impacts to occur, such as short-term impacts from construction traffic and noise. It is assumed that, as the Proposed Development, all construction projects would be carried out in line with inherent environmental controls, regulatory controls, and best practice measures, and that larger developments will have carried out environmental assessments for the respective developments. In addition, as other permitted developments within 1km of the site have finished construction, WCC Ref. no. 24252 is the only development likely to result in cumulative impacts, should its construction phase overlap with that of the Proposed Development. Taking this into consideration, as well as the type, location, size, and scale of the Proposed Development and other approved developments in the surrounding environs, and with the implementation of the Contractor's CEMP on all sites, no significant cumulative impacts are likely to occur during the construction phase.</p> <p>During the operational phase, other permitted developments are likely to result in an increase in traffic and congestion in the area. The Proposed Development has the potential to reduce the effects of this increase and support the sustainable development of the town by providing new set-down which will support the improvement of bus links, and new active travel facilities which will link community services and residential and employment areas.</p>
(h) the possibility of effectively reducing the impact	
<i>What measures can be adopted to avoid, reduce, repair or compensate the impact?</i>	<p>The Proposed Development is unlikely to result in any significant effects. Where transient or short-term impacts are likely to occur, the implementation of embedded or appropriate construction mitigation measures will avoid or reduce the likelihood, duration, and/or intensity of impacts on the receiving environment. During the construction phase, the risk of pollution events, accidents, and/or nuisances will be avoided or reduced through the implementation of appropriate mitigation measures such as those to be outlined in the Contractor's CEMP, CTMP, and RWMP, as well as adherence to appropriate national guidelines and codes of practice. The Contractor's CEMP will include, but not be limited to, the following mitigation measures:</p> <ul style="list-style-type: none"> • Clearing or felling of vegetation will be carried out outside of the bird breeding season (i.e., generally accepted to be March to August inclusive), and a suitably

Criteria

Commentary

- qualified and experienced ecologist will carry out checks for birds immediately prior to these activities. Each new construction/felling area will be checked not more than 72 hours prior to commencement of works as nests can be quickly established. Where any active nests are identified, suitable exclusion zone(s) will be established and machinery, personnel, and materials will be excluded from these zones until the ecologist determines that the breeding attempt(s) have concluded;
- Targeted ecological field searches will be carried out by a suitably qualified and experienced ecologist to check for protected and/or notable habitats and species which may be impacted by construction works, and suitable and proportionate mitigation measures will be implemented to avoid or reduce these impacts, where possible;
 - Trees and scrub clearing or felling will be kept to a minimum;
 - All personnel involved in the construction of the Proposed Development will be made aware of the ecological features present in the surrounding environs of the Proposed Development and sightings of protected or notable species within the site or its immediate surrounding environs will be recorded. If any evidence or sightings of protected or notable species occur within the site, work in that area will cease then and advice will be sought from the appointed Ecological Clerk of Works (ECoW);
 - At no point will water be abstracted or discharged from surface or ground waterbodies;
 - Hardstanding areas and surface roads will be regularly cleaned to avoid waste or pollutants being washed into nearby surface waterbodies and designated sites;
 - Appropriate mitigation measures, such as silt fences/barriers, will be utilised to avoid surface runoff reaching nearby surface waterbodies. The selection of the appropriate measures and their location will take into consideration that elevation decreases from the Proposed Development site towards the east, in the direction of nearby surface waterbodies and designates sites;
 - All plant and equipment will be refuelled and maintained at a designated location away from disturbed/excavated soils, preferably off-site, where proper control measures can be employed in the event of a spill or leak;
 - Storage of fuel and oil onsite, and the procedures to be followed during delivery and transferring of fuel, will be controlled;
 - Effluent from washing of vehicles, equipment, and other structures onsite will be discharged to the foul sewer (with the consent of the relevant provider) and/or collected in a sealed tank for removal off-site by a licensed waste disposal contractor;
 - Waste and materials with the potential to contaminate soil and/or water resources will be stored in designated areas protected against spillage or leachate runoff;
 - Excavations will be kept to a minimum and soils will be reused onsite, where possible;
 - Temporary works (e.g., physical barriers) will be in place to prevent runoff from entering disturbed/excavated soils;
 - Mitigation measures will be implemented to avoid or reduce dust emissions, particularly in proximity to sensitive receptors such as receptors at the East Glendalough School;
 - A protocol to manage unexpected archaeological finds will be included and implemented should any be found during the construction phase. Work in that area will cease, the area will be protected, and further archaeological investigation shall be carried out by hand under the supervision of the appointed Archaeologist to determine the nature and extent of archaeological remains. Any archaeological mitigation must be agreed in consultation with the National Monuments Service (NMS) and WCC;
 - Noise and vibration impacts will be managed through adherence to site working hours as agreed with WCC, which are anticipated to be as described in Section 5.5 of this report. In addition, the Contractor shall adhere to noise emission limits as agreed with WCC and to be outlined within the Contractor's CEMP;
 - Construction material shall be locally sourced from licensed suppliers, where possible;
 - A public notice will be published ahead of train service disruptions;
 - Prior to the commencement of the works, utility surveys will be updated and supplied to the Contractor along with any other relevant site investigations; and
 - To avoid or reduce impacts on the existing road network, particularly where congestion is known to occur along Station Road, construction site compounds will be located in the greenfield land north of the railway line. Traffic to and from the site, including movement of construction staff and Heavy Goods Vehicles (HGV), will be managed through the Contractor's CTMP as agreed with WCC. Where possible, HGV movements will be outside of peak traffic hours.

Criteria

Commentary

In addition, during the operational phase, the Applicant will replace any proposed planting that is diseased or fails to grow within 2 years of planting, or any existing vegetation that becomes damaged or diseased as a result of construction works.

From an assessment of the types and characteristics of the potential impacts likely to arise from the Proposed Development based on the information available at the time of writing this report, it is considered it will not constitute EIA development. With the implementation of the control measures included in the Contractor's CEMP, CTMP, RWMP, few impacts will be likely to arise. Should any impacts arise, they are likely to be transient or short-term, restricted to the Proposed Development site and a limited area in proximity, and unlikely to be significant. During the operational phase, the Proposed Development will encourage a modal shift from the use of private vehicles to active travel and public transport, reducing congestion in the area and supporting the sustainable development of the Key Town of Wicklow.

7 Conclusion

The prescribed classes of development and thresholds that trigger a full EIA are set out in Schedule 5 of the Planning and Development Regulations, 2001 (as amended). A review of the project types listed in the aforementioned Schedule 5 (as amended) has been carried out, using the steps set out in Section 3 of this report.

The Proposed Development is not a type of development listed in Schedule 5, Part 1 and as the Proposed Development does not equal or exceed a development of a type listed in Part 2 of Schedule 5, a full EIA culminating in the preparation of an EIAR is not required. In addition, the Proposed Development is not a type of development requiring a mandatory EIA under the Roads Act 1993 (as amended).

The Proposed Development is of a class set out in Schedule 5, Part 2 (Schedule 5, Part 2, 10 (b)(iv)), but does not meet or exceed the relevant threshold. The Proposed Development was screened for EIA in line with Schedule 7 and 7A of the Planning and Development Regulations 2001 (as amended). No likely significant effects have been identified during the screening process and, as such, a full EIA culminating in the preparation of an EIAR is not required.

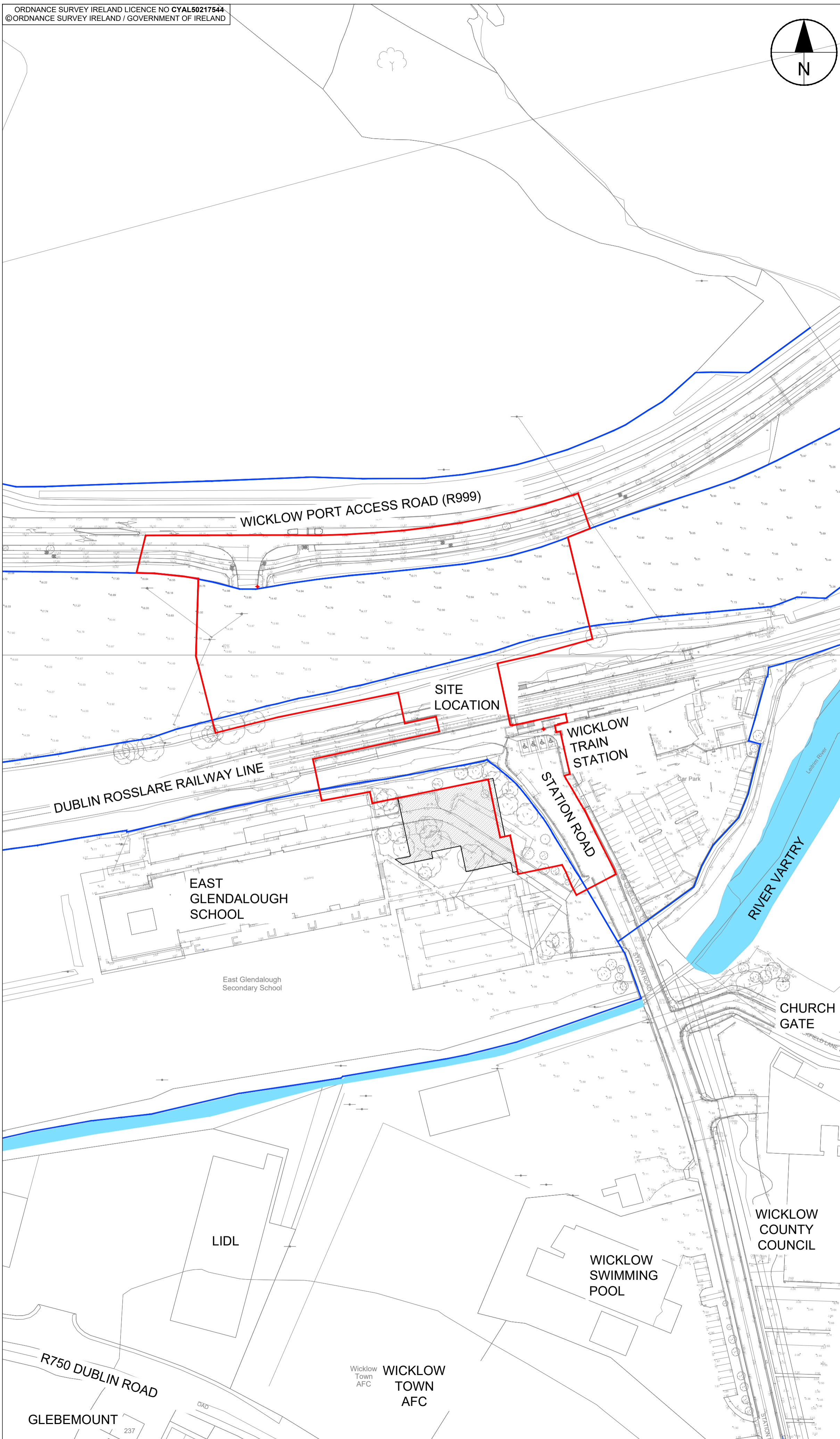
A screening checklist is included in Appendix C of this report.

8 References

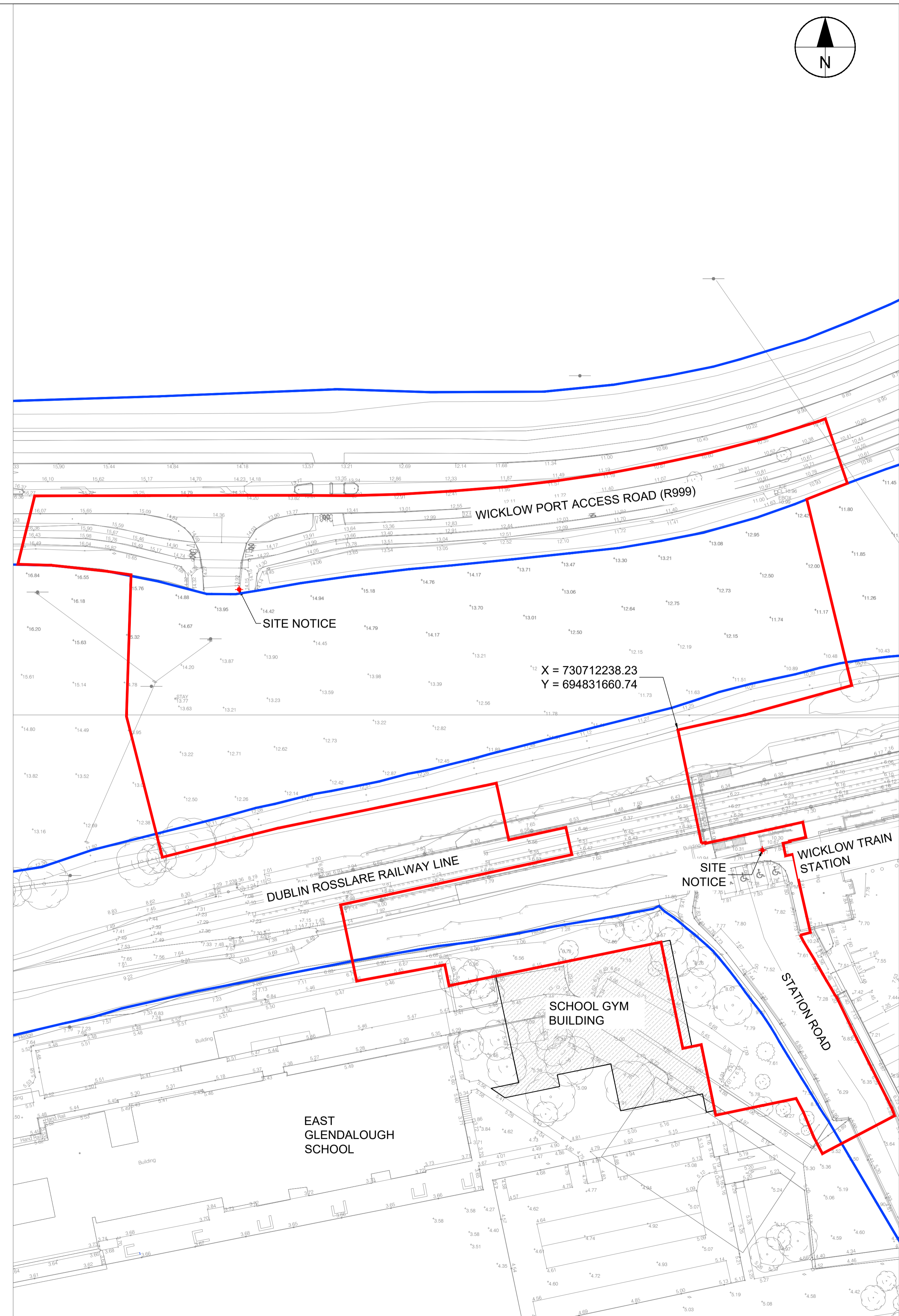
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Appendix A : Drawings

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 Project Management Initials: Designer: CM Checked: AC Approved: NR ISO A1 594mm x 841mm




LOCATION PLAN
Scale 1:1000




SITE BOUNDARY
Scale 1:500

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PROJECT
WICKLOW PAR LINK

CLIENT



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LEGEND

LAND BOUNDARY —

SITE BOUNDARY —

LOCATION OF SITE NOTICE ◆

ISSUE/REVISION

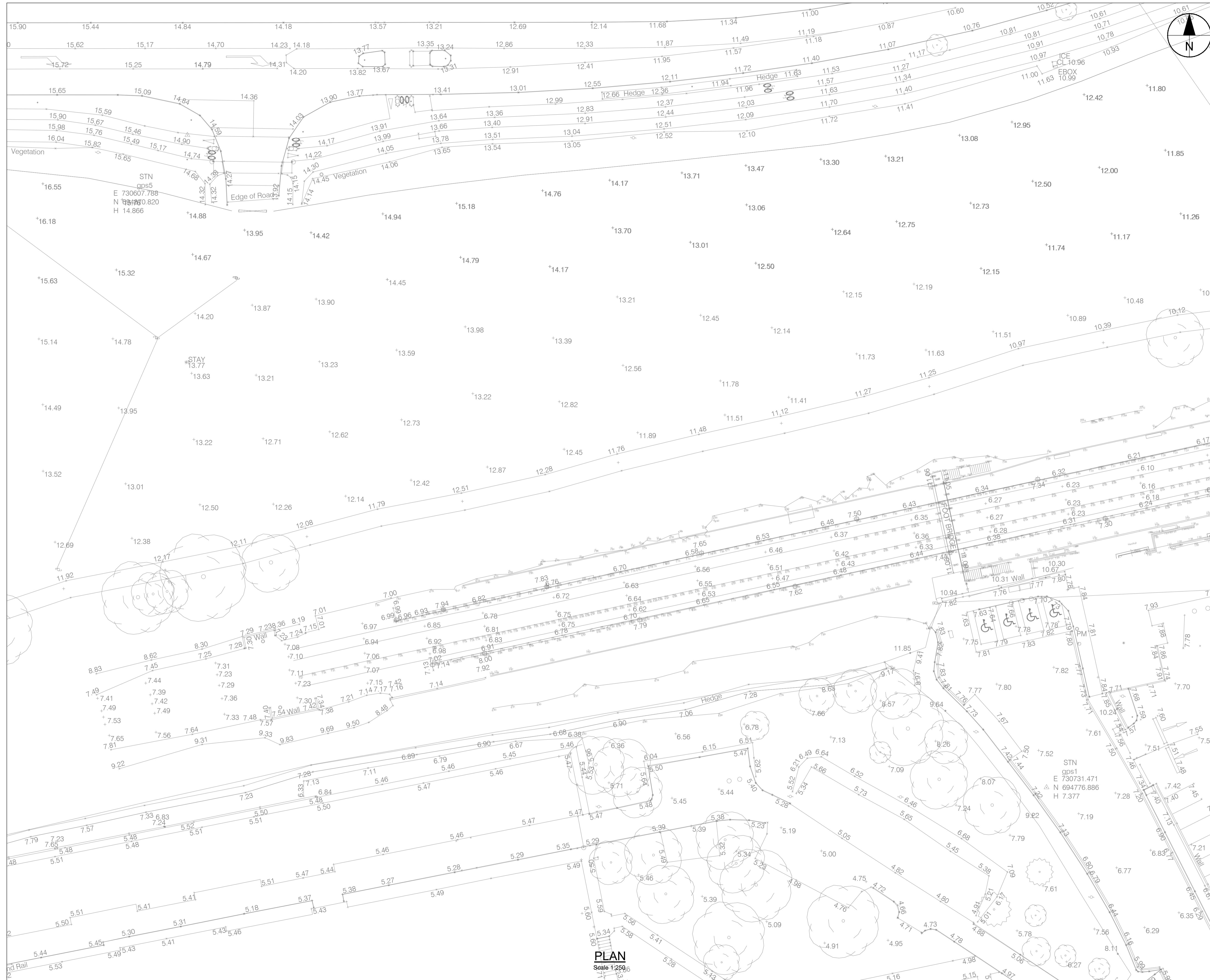
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60647343

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SHEET NUMBER
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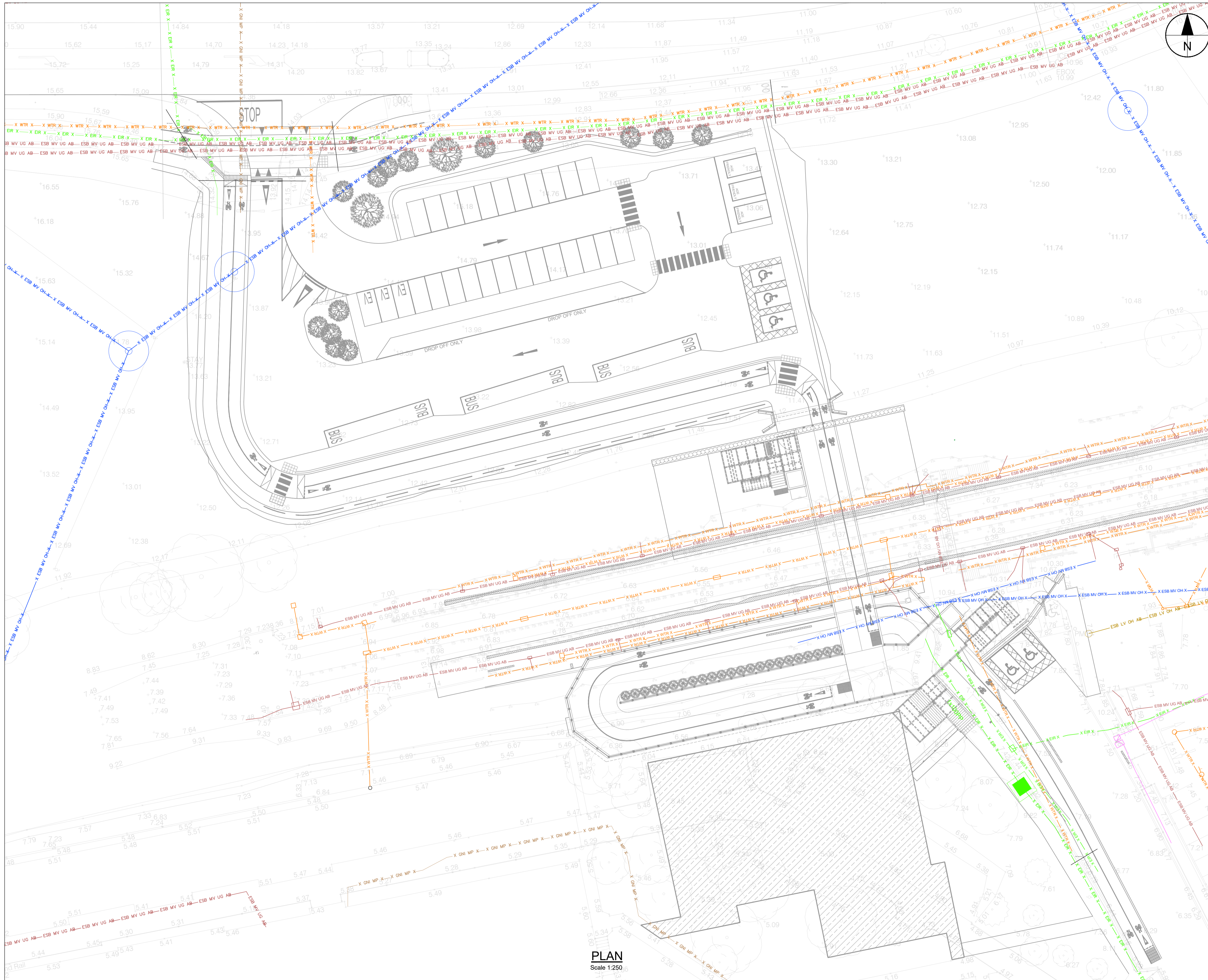
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P01	OCT 2024	ISSUED FOR PLANNING
I/R	DATE	DESCRIPTION


PROJECT NUMBER
60647343

SHEET TITLE
WICKLOW PAR LINK
TOPOGRAPHICAL SURVEY


SHEET NUMBER
WPAR-ACM-GEN-SW-DR-ZZ-0002



PLAN
 Scale 1:250



PROJECT
 WICKLOW PAR LINK

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 Údarás Náisiúnta Iompair
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LEGEND

- X WTR X EXISTING WATER
- X GNI MP X EXISTING GAS
- X EIR X EXISTING EIR
- X VM X EXISTING VIRGIN MEDIA
- X ESB MV OH X EXISTING ESB MV OVERHEAD
- ESB LV OH AB EXISTING ESB LV OVERHEAD
- ESB MV UG AB EXISTING ESB MV/LV UNDERGROUND

ISSUE/REVISION

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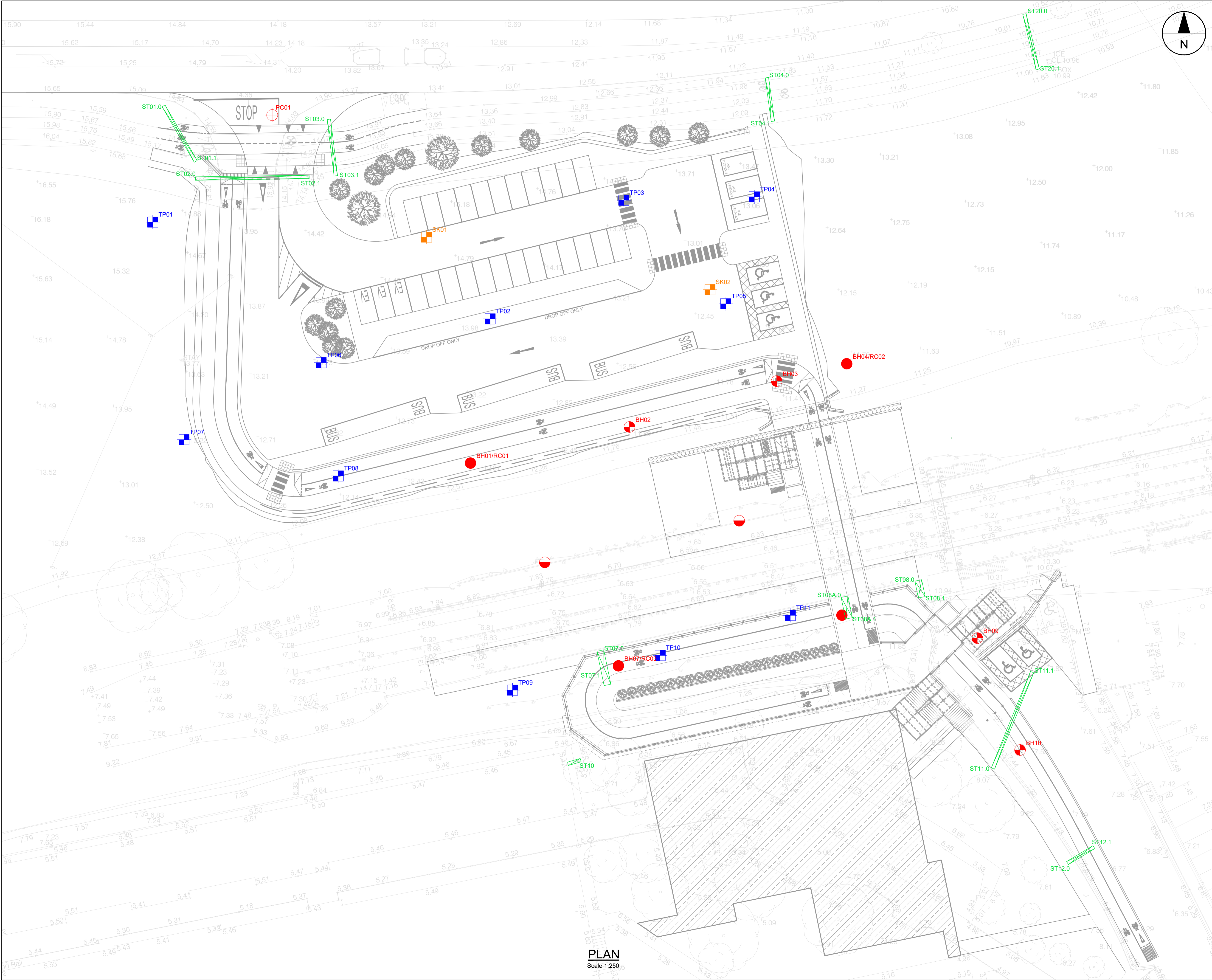
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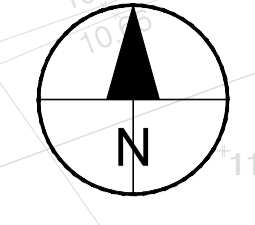
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PLAN
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LEGEND

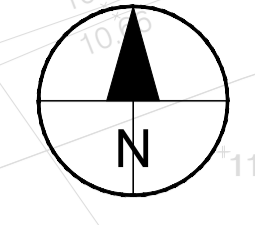
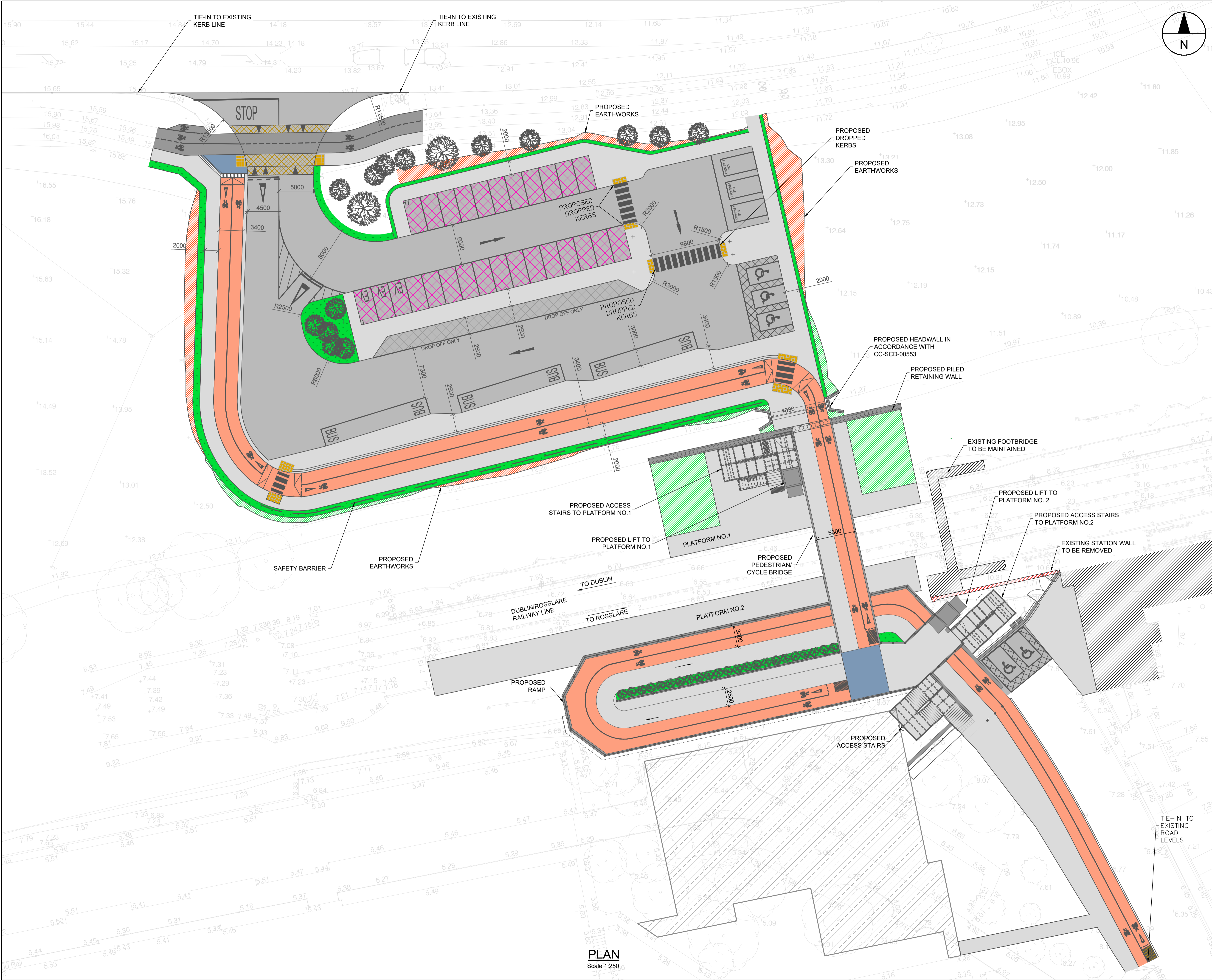
	TRIAL PIT
	BOREHOLE
	BOREHOLE & ROTARY CORE
	WINDOW SAMPLE & DYNAMIC PROBE
	PAVEMENT CORE
	SOAKAWAY
	SLIT TRENCH

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File name: L:\EGAC\Y\IED\B\FP001\DATA\DCS\PROJECTS\CI060647343_WPARLINK900_CAD_GIS\910_CAD\30 - SHEETS\WPAR-ACM-HGN-SW-DR-CH-1000-P01 GENERAL ARRANGEMENT.DWG
 Last saved by: RODGERSON Last Plotted: 2024-10-03
 ISO A1 594mm x 841mm
 Approved: NR
 Checked: CC
 Designer: CE
 Project Management Initials:



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LEGEND

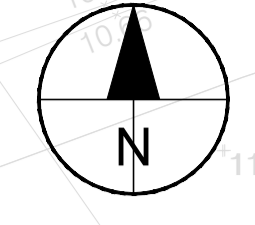
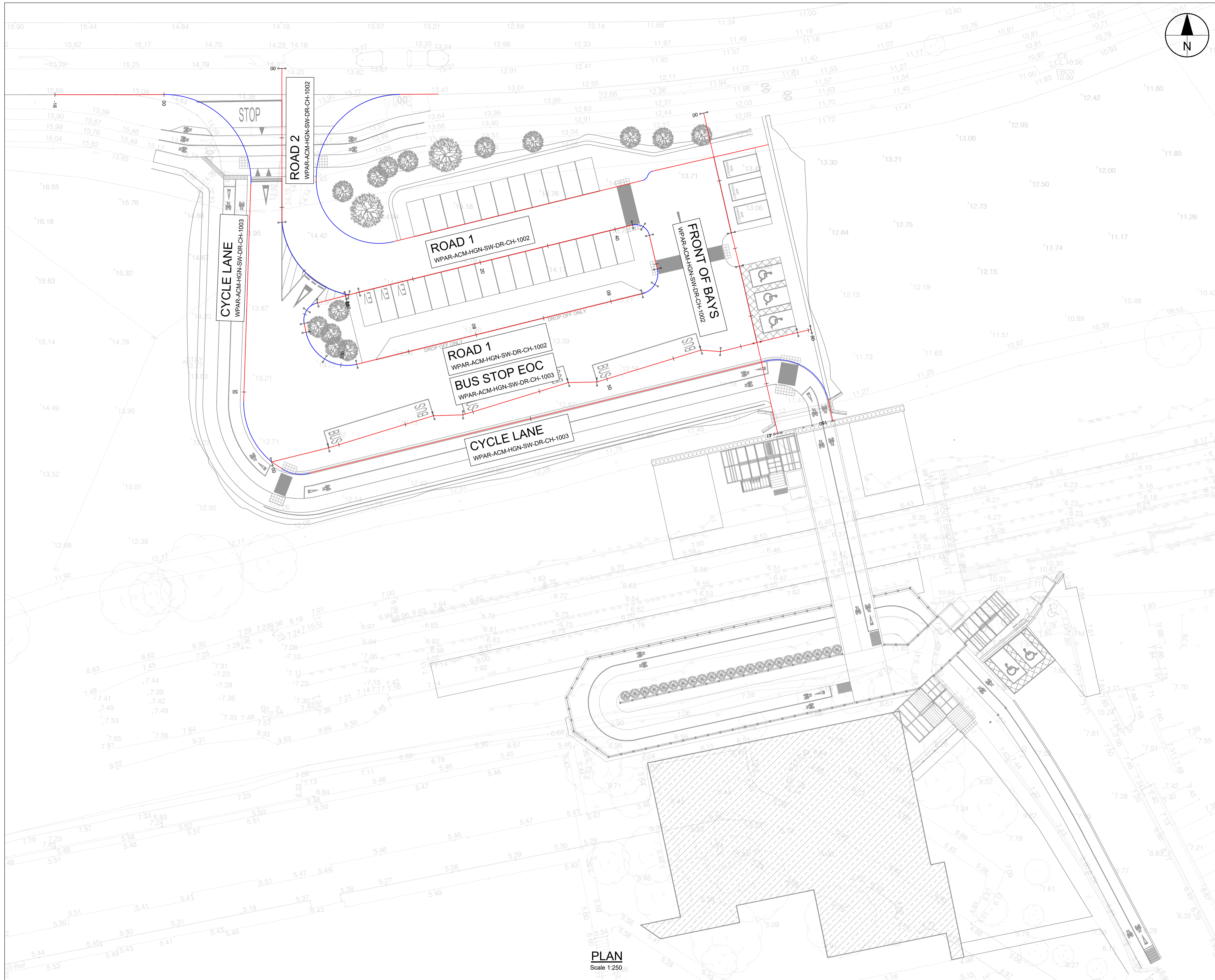
	PROPOSED ROAD
	PROPOSED FOOTPATH
	PROPOSED CYCLE TRACK
	PROPOSED PORT ACCESS ROAD CYCLE TRACK
	PROPOSED SHARED AREA
	PROPOSED RAISED RAMP
	PROPOSED DROP OFF SPACES
	PROPOSED SET-DOWN SPACES
	PROPOSED VERGE/LANDSCAPING
	PROPOSED EMBANKMENT
	PROPOSED CUTTING
	CORDUROY HAZARD PAVING
	UNCONTROLLED CROSSING TACTILE PAVING

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PLAN
Scale 1:250

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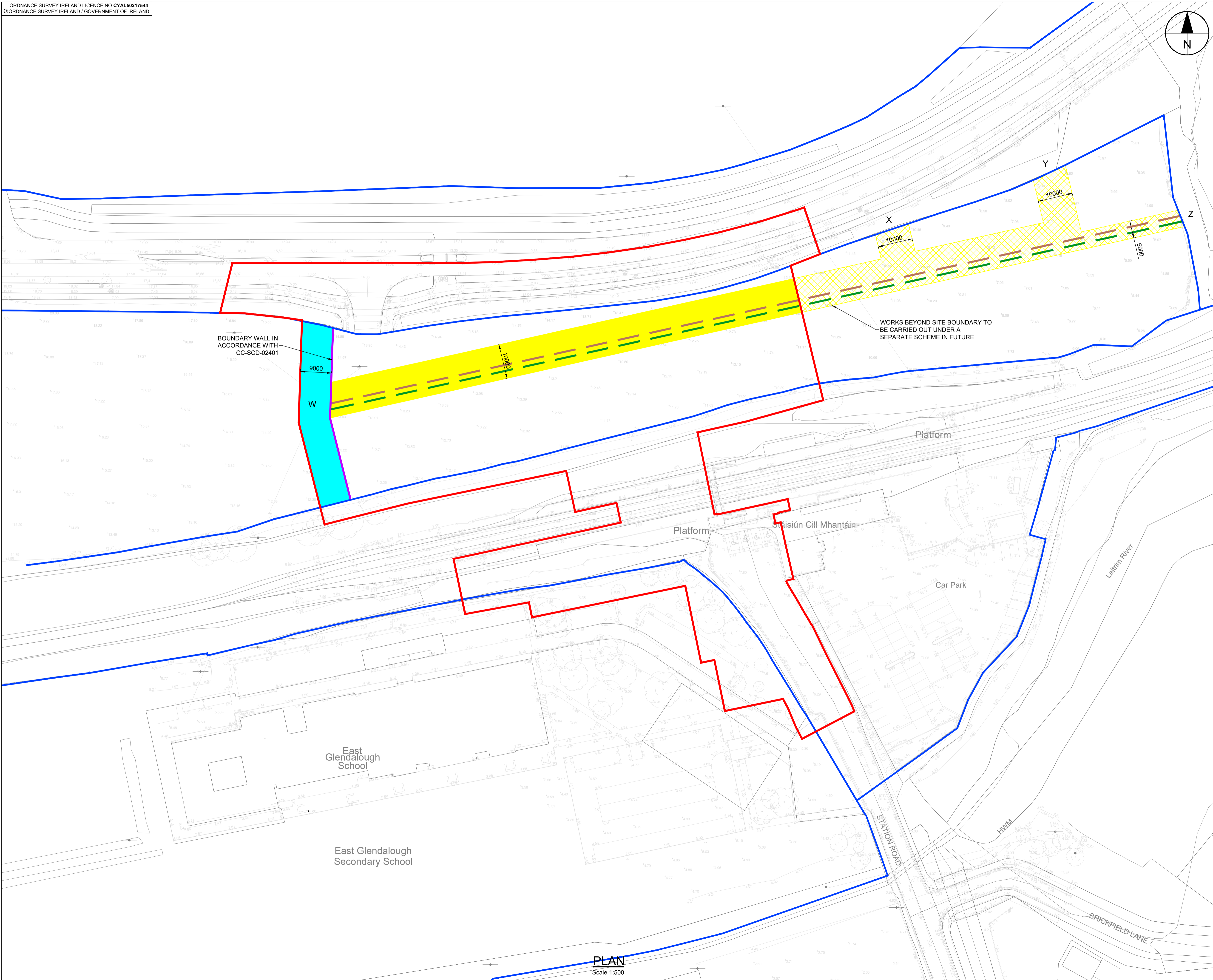


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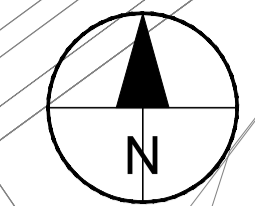
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PLAN
 Scale 1:500



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LEGEND

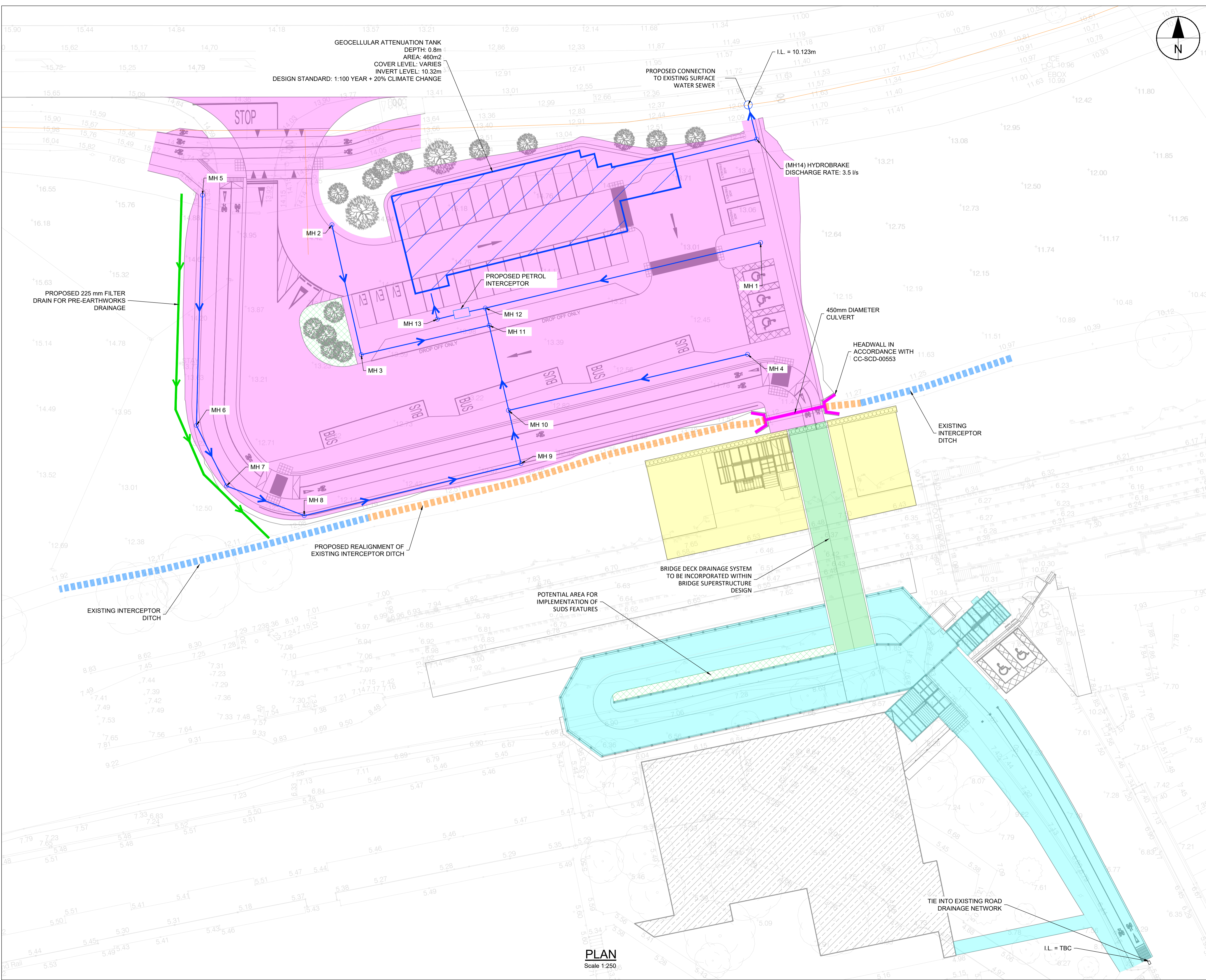
—	SITE BOUNDARY
—	LAND BOUNDARY
—	FUTURE SURFACE WATER SEWER
—	FUTURE FOUL SEWER CONNECTION
—	BOUNDARY WALL
■	TEMPORARY LANDTAKE
■	WAYLEAVE
▨	FUTURE WAYLEAVE

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File name: L:\LEGACY\IED\B\2\F001\DATA\DCS\PROJECTS\CI160647343_WPARLINK\900_CAD_GIS\910_CAD\300 - SHEETS\WPAR-ACM-HGN-SW-DR-CH-2001-P01_DRAINAGE.DWG
 Last saved by: COSTELLO Last Plotter: 2024-10-03
 Project Management Initials: Designer: SH Checked: PS Approved: NR ISO A1 594mm x 841mm



PROJECT
WICKLOW PAR LINK

CLIENT



CONSULTANT
 AECOM
 4th FLOOR ADELPHI PLAZA
 GEORGE'S STREET UPPER
 DUN LAOGHAIRE, CO. DUBLIN
 Tel: +353-1-238-3100
 www.aecom.com

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LEGEND

	CATCHMENT AREA 1
	CATCHMENT AREA 2
	CATCHMENT AREA 3
	BRIDGE DECK
	GRASSED/SUDS AREA
	EXISTING STORM WATER DRAIN
	PROPOSED STORM WATER DRAIN
	PROPOSED DRAINAGE PIPE
	PROPOSED ATTENUATION TANK
	PROPOSED FILTER DRAIN

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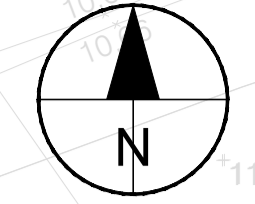
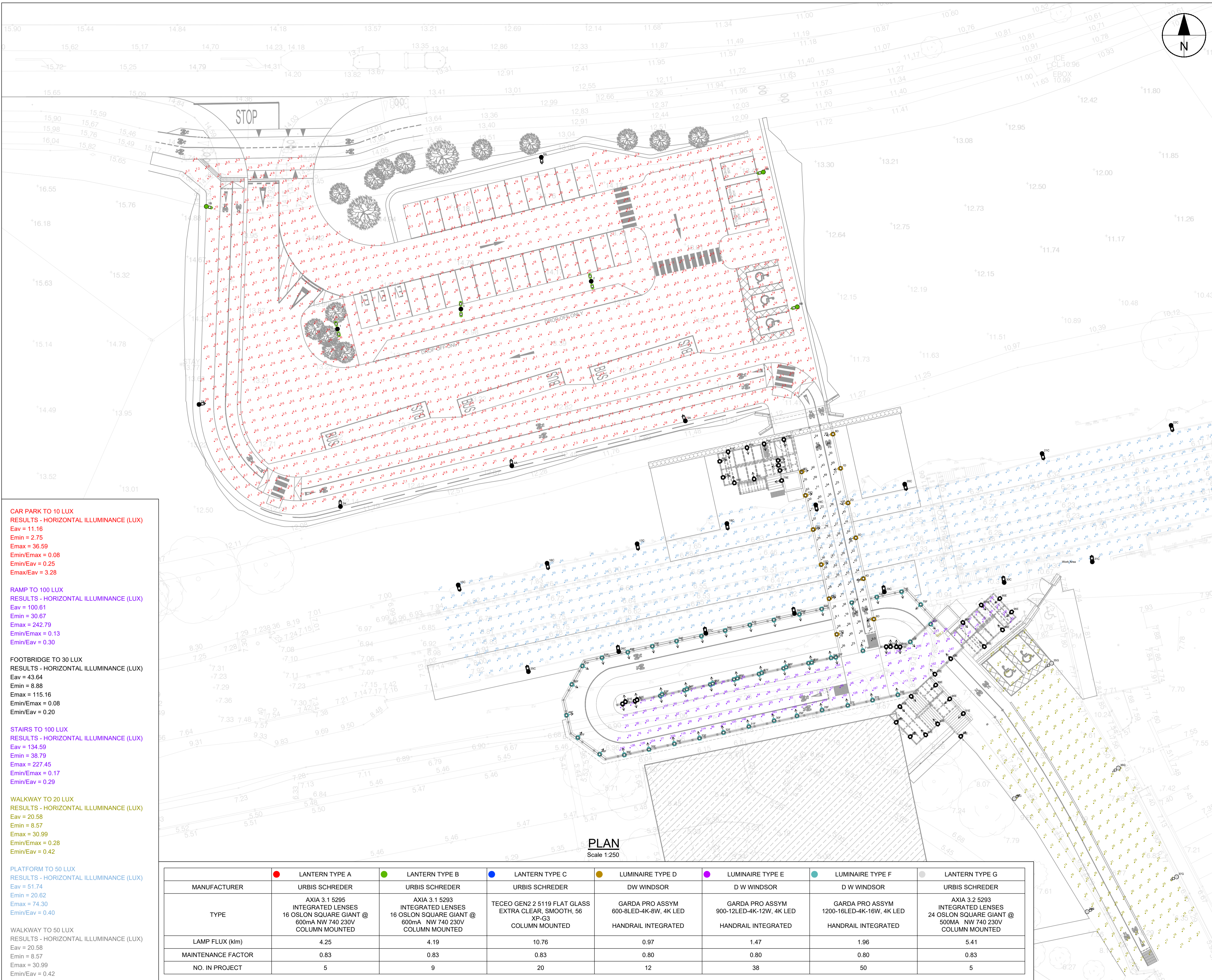
PROJECT NUMBER
60647343

SHEET TITLE
WICKLOW PAR LINK DRAINAGE

SHEET NUMBER
WPAR-ACM-HGN-SW-DR-CH-2001

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 Last saved by: FRANK KELLY Last Plotter: 2024-10-03
 ISO A1 594mm x 841mm
 Approved: NR
 Checked: NR
 Designer: FK
 Project Management Initials:



AECOM

PROJECT
WICKLOW PAR LINK

CLIENT



CONSULTANT
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 4th FLOOR ADELPHI PLAZA
 GEORGE'S STREET UPPER
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- LEGEND**
- LANTERN TYPE A
 - LANTERN TYPE B
 - LANTERN TYPE C
 - LUMINAIRE TYPE D
 - LUMINAIRE TYPE E
 - LUMINAIRE TYPE F
 - MAST (TWIN LANTERN)
 - BEAM AIMING TARGET
 - PHOTOMETRIC CENTRE

ISSUE/REVISION

NO.	DATE	DESCRIPTION
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PROJECT NUMBER
60647343

SHEET TITLE
WICKLOW PAR LINK
PUBLIC LIGHTING

SHEET NUMBER
WPAR-ACM-HLG-SW-DR-EO-1001

CAR PARK TO 10 LUX
 RESULTS - HORIZONTAL ILLUMINANCE (LUX)
 Eav = 11.16
 Emin = 2.75
 Emax = 36.59
 Emin/Emax = 0.08
 Emin/Eav = 0.25
 Emax/Eav = 3.28

RAMP TO 100 LUX
 RESULTS - HORIZONTAL ILLUMINANCE (LUX)
 Eav = 100.61
 Emin = 30.67
 Emax = 242.79
 Emin/Emax = 0.13
 Emin/Eav = 0.30

FOOTBRIDGE TO 30 LUX
 RESULTS - HORIZONTAL ILLUMINANCE (LUX)
 Eav = 43.64
 Emin = 8.88
 Emax = 115.16
 Emin/Emax = 0.08
 Emin/Eav = 0.20

STAIRS TO 100 LUX
 RESULTS - HORIZONTAL ILLUMINANCE (LUX)
 Eav = 134.59
 Emin = 38.79
 Emax = 227.45
 Emin/Emax = 0.17
 Emin/Eav = 0.29

WALKWAY TO 20 LUX
 RESULTS - HORIZONTAL ILLUMINANCE (LUX)
 Eav = 20.58
 Emin = 8.57
 Emax = 30.99
 Emin/Emax = 0.28
 Emin/Eav = 0.42

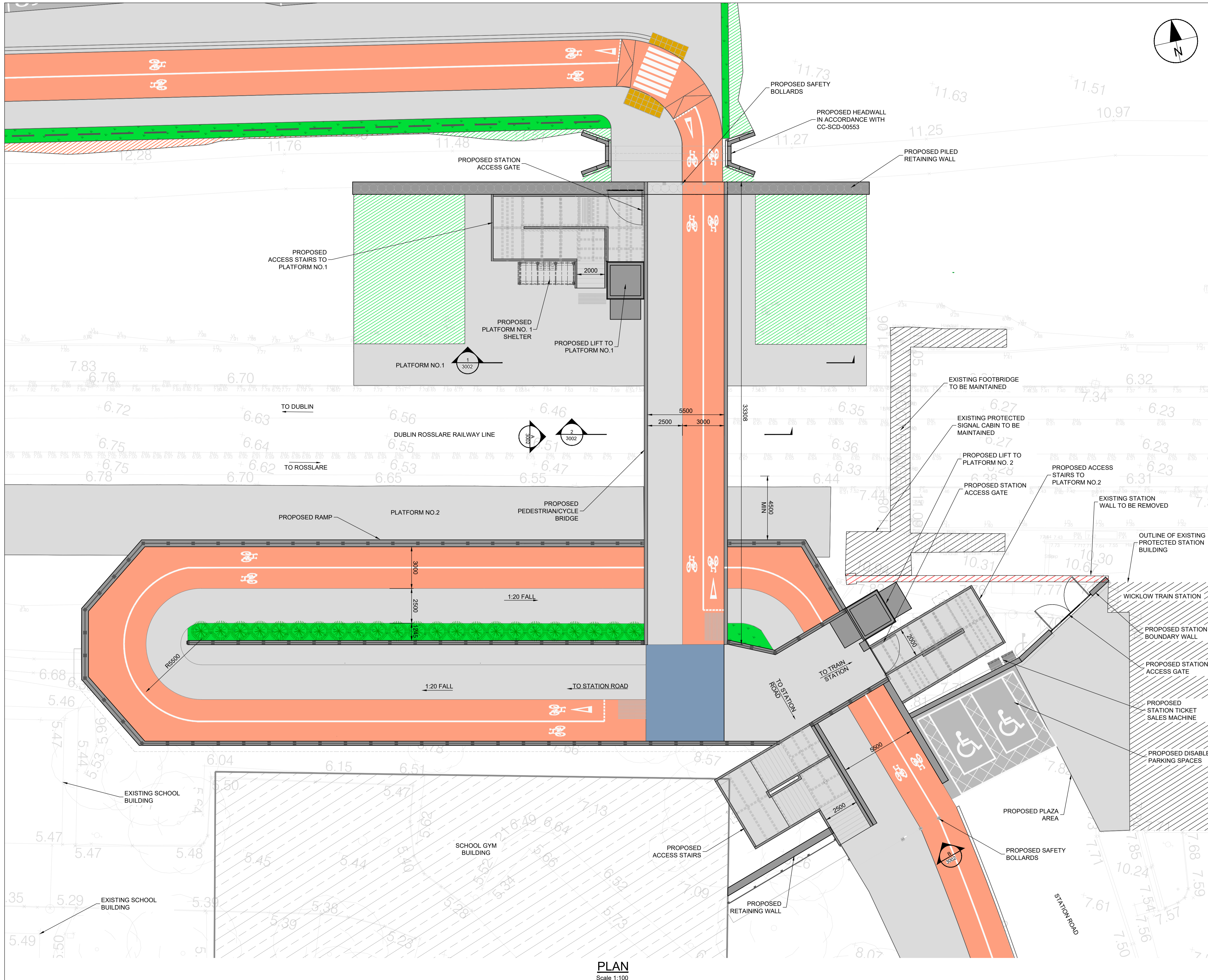
PLATFORM TO 50 LUX
 RESULTS - HORIZONTAL ILLUMINANCE (LUX)
 Eav = 51.74
 Emin = 20.62
 Emax = 74.30
 Emin/Eav = 0.40

WALKWAY TO 50 LUX
 RESULTS - HORIZONTAL ILLUMINANCE (LUX)
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 Emin = 8.57
 Emax = 30.99
 Emin/Emax = 0.28
 Emin/Eav = 0.42

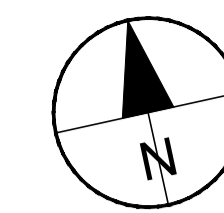
PLAN
Scale 1:250

	● LANTERN TYPE A	● LANTERN TYPE B	● LANTERN TYPE C	● LUMINAIRE TYPE D	● LUMINAIRE TYPE E	● LUMINAIRE TYPE F	● LANTERN TYPE G
MANUFACTURER	URBIS SCHREDER	URBIS SCHREDER	URBIS SCHREDER	DW WINDSOR	D W WINDSOR	D W WINDSOR	URBIS SCHREDER
TYPE	AXIA 3.1 5295 INTEGRATED LENSES 16 OSLOM SQUARE GIANT @ 600mA NW 740 230V COLUMN MOUNTED	AXIA 3.1 5293 INTEGRATED LENSES 16 OSLOM SQUARE GIANT @ 600mA NW 740 230V COLUMN MOUNTED	TECEO GEN2 2 5119 FLAT GLASS EXTRA CLEAR, SMOOTH, 56 XP-G3 COLUMN MOUNTED	GARDA PRO ASSYM 600-8LED-4K-8W, 4K LED HANDRAIL INTEGRATED	GARDA PRO ASSYM 900-12LED-4K-12W, 4K LED HANDRAIL INTEGRATED	GARDA PRO ASSYM 1200-16LED-4K-16W, 4K LED HANDRAIL INTEGRATED	AXIA 3.2 5293 INTEGRATED LENSES 24 OSLOM SQUARE GIANT @ 500mA NW 740 230V COLUMN MOUNTED
LAMP FLUX (klm)	4.25	4.19	10.76	0.97	1.47	1.96	5.41
MAINTENANCE FACTOR	0.83	0.83	0.83	0.80	0.80	0.80	0.83
NO. IN PROJECT	5	9	20	12	38	50	5

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PLAN
 Scale 1:100



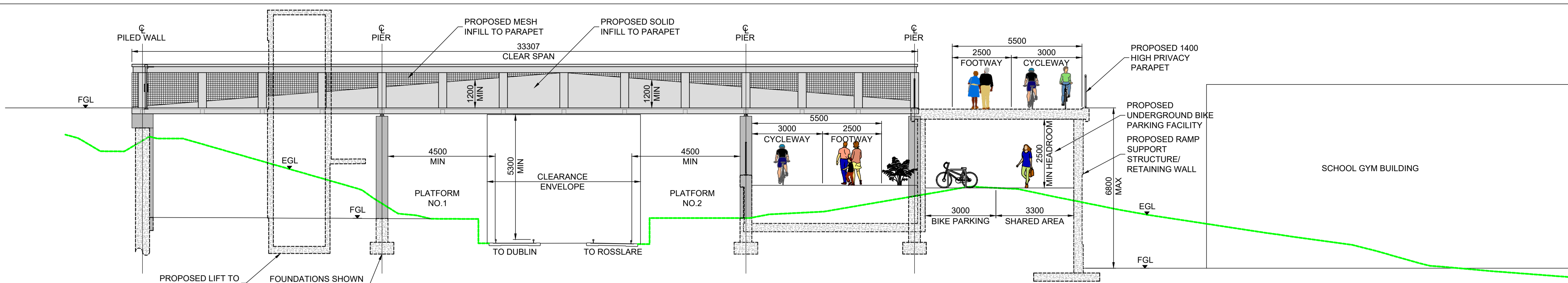
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- PROPOSED STRUCTURE
- PROPOSED FOOTPATH
- PROPOSED CYCLE TRACK
- PROPOSED ROAD/PLAZA
- PROPOSED SHARED AREA
- PROPOSED VERGE/LANDSCAPING
- PROPOSED EMBANKMENT
- PROPOSED CUTTING
- CORDUROY HAZARD PAVING

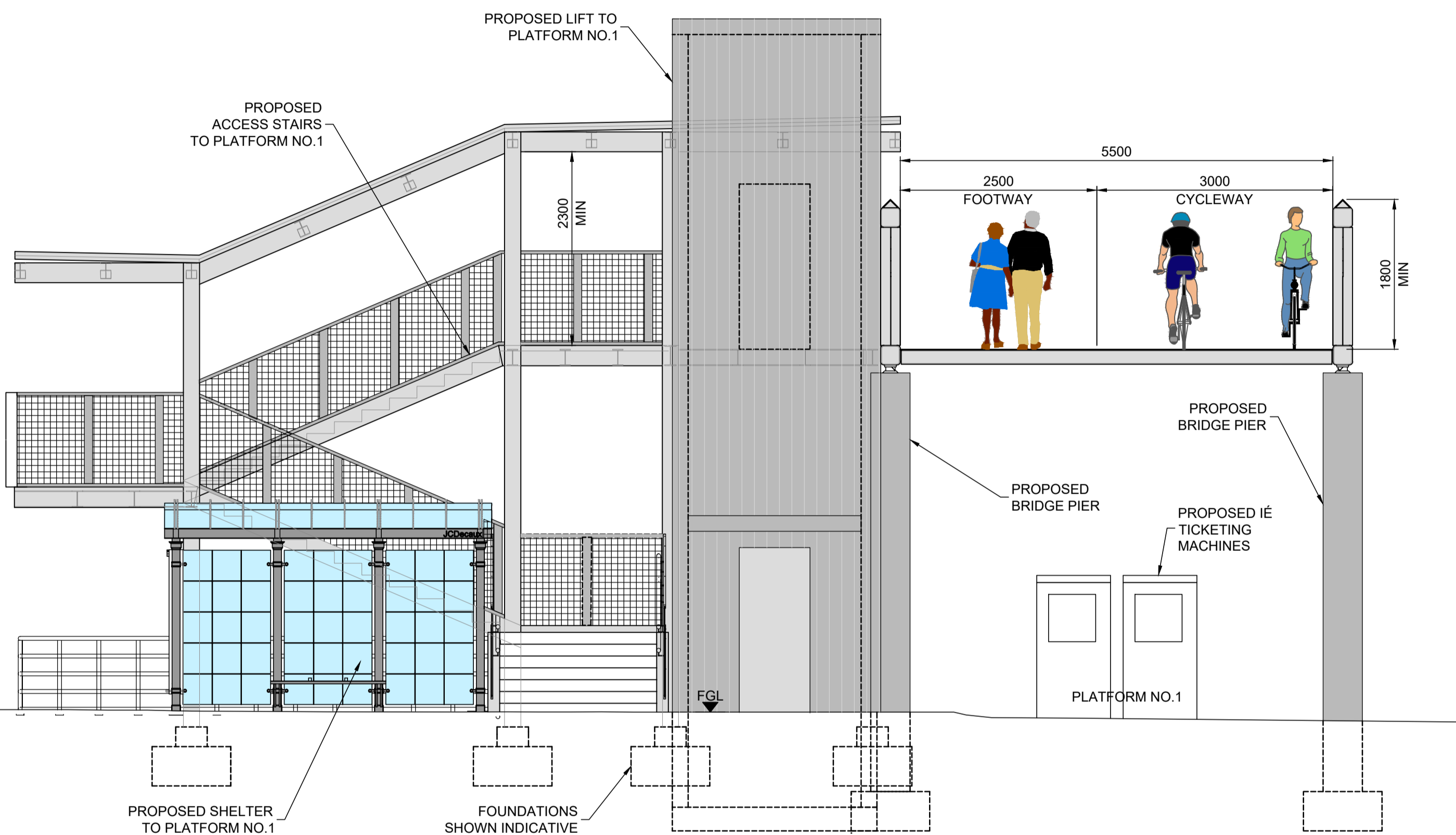
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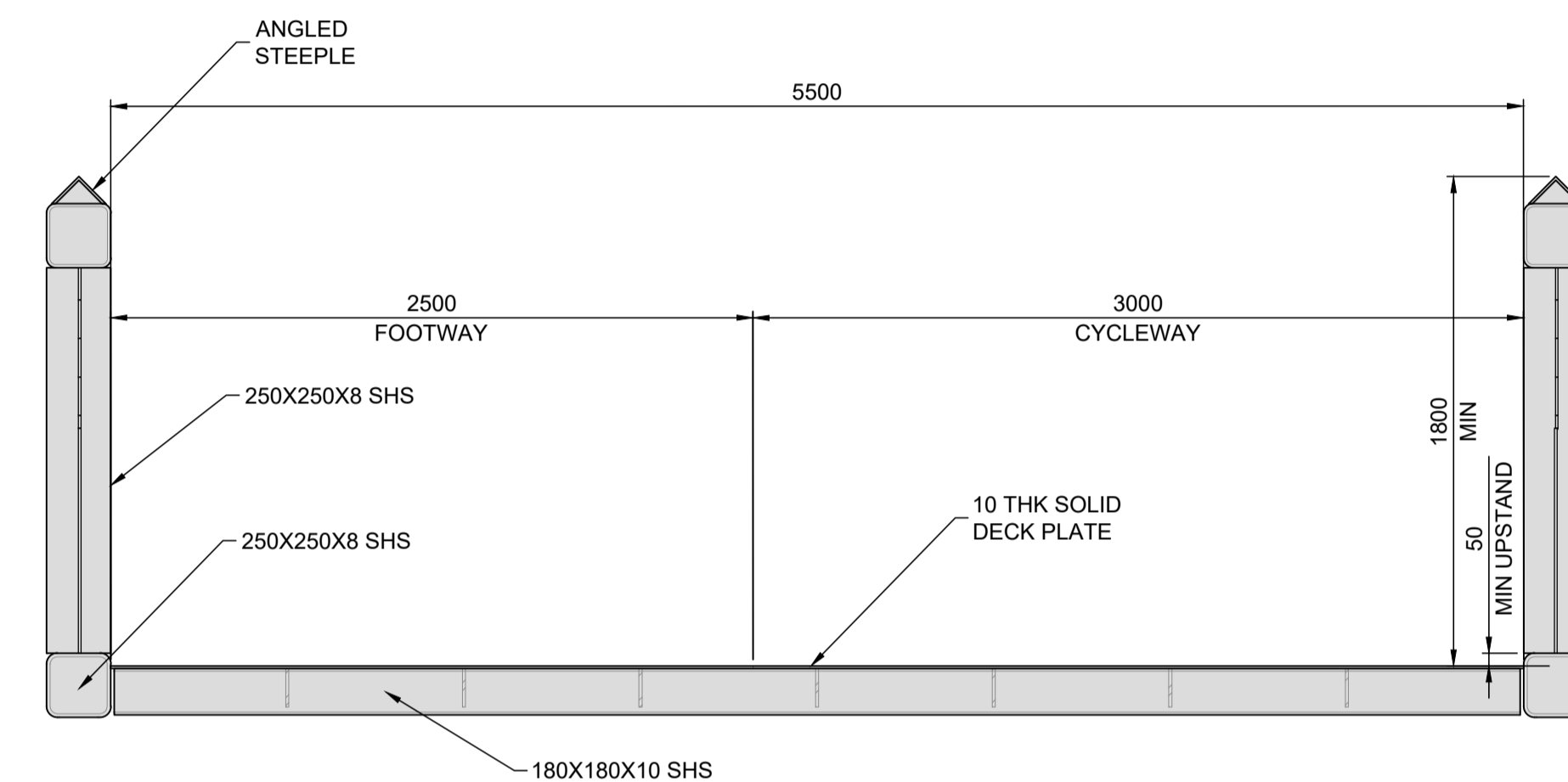
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 Last saved by: COSTELLOA Last Potted: 2024-10-03
 ISO A1 594mm x 841mm
 Approved: NR
 Checked: AC
 Designer: CM
 Project Management Initials:



ELEVATION A-A BRIDGE ELEVATION
Scale 1:100



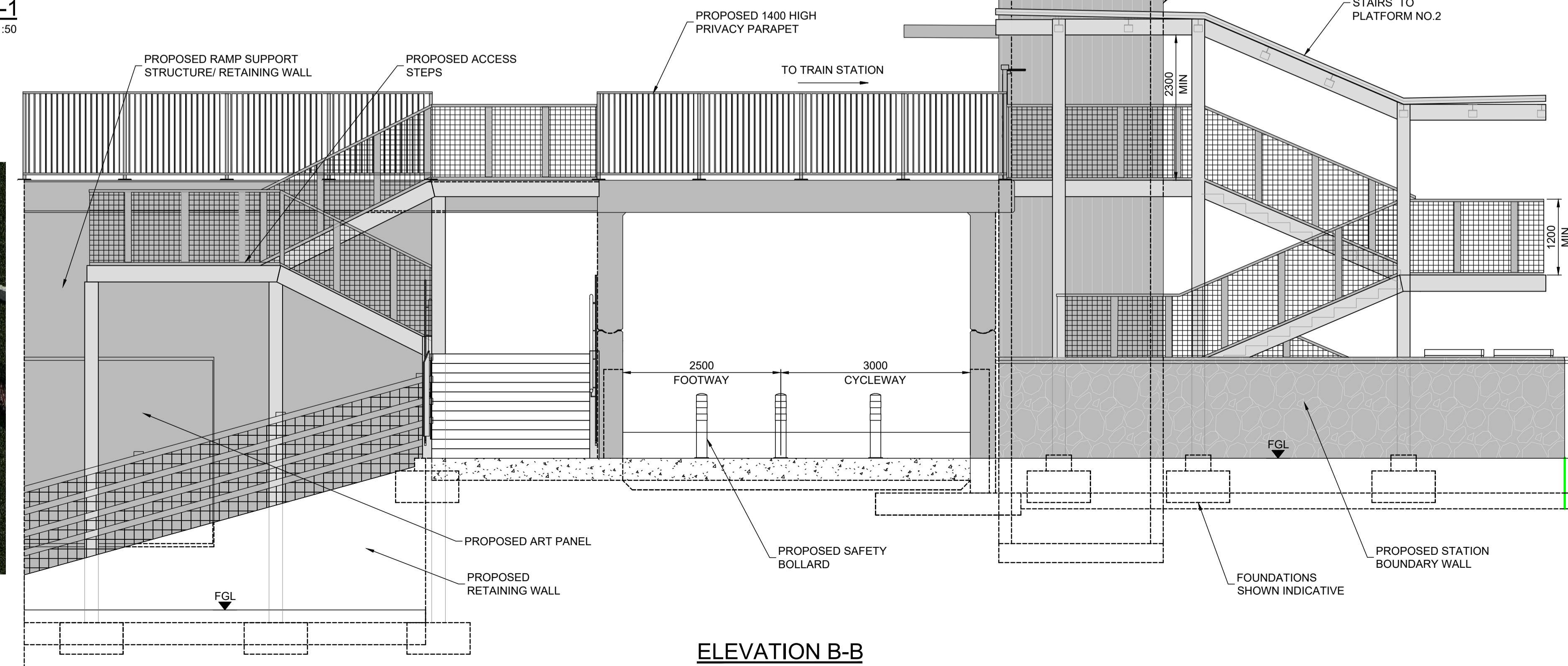
SECTION 1-1
Scale 1:50



SECTION 2-2
Scale 1:50



3D VIEW
Scale NTS



ELEVATION B-B
Scale 1:50



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Appendix B : Planning Search

Planning Authority	Ref. no.	Address	Summary of Proposed Development	Grant date	Distance from Site (approx.)
WCC	19211	East Glendalough School, Station Road, Wicklow Town, Co. Wicklow	Provision of a 1,350m ² , 8.85m high sports hall building, ancillary spaces, and all associated site works to the east of the existing school building.	05/11/2019	Within
WCC	24252	Coral Leisure, Station Road, Wicklow Town, Co. Wicklow	Under Part 8 for proposed gym extension. The proposed extension is 380m ² gross internal floor area (GiFA). The extension will comprise the extended village changing area, gymnasium and aerobics room. The gymnasium extension and the Aerobics space will replace the existing prefabricated building. The existing prefabricated unit, which has a gross internal floor area of 165m ² , will be removed. The extension will add a further 211m ² of gross internal floor area on the site, following the removal of the prefab. The proposed extension will be finished in a metal composite material, with rendered blockwork and Paralon roof membrane, or similar. The palette of finishes proposed will be in keeping with the existing building. Aluminium framed glazing and door frames will be dark grey in colour. External treatments will include pressed metal balustrade with handrail and concrete footpaths.	16/09/2024	190m
WCC	191193	Lidl, Dublin Road, Wicklow Town, Co. Wicklow	Construction of a new Discount Foodstore Supermarket with ancillary off-licence sales. The proposed development comprises: <ol style="list-style-type: none"> 1. The demolition of existing single storey Discount Foodstore (with ancillary off-licence use) measuring 1,738m² gross floor space with a net retail sales area of 1,286m²; 2. The construction of a 2-storey mono-pitch roofed Discount Foodstore (with ancillary off-licence use) measuring 2,485m² gross floor space with a net retail sales area of 1,650m²; 3. Redevelopment/reconfiguration of existing site layout and car parking; and 4. Provision of free standing and building mounted signage, free standing trolley bay and enclosure, hard and soft landscaping, public lighting, electric vehicle charging infrastructure, roof mounted solar panels, surface water drainage infrastructure, cycle parking, modified pedestrian entrance, modified boundary treatments, connections to services and all other associated ancillary development and works above and below ground level. 	31/01/2020	370m
ABP	306321	Dublin Road & Church Hill Road, Wicklow Town, Co. Wicklow	Demolition of structures and boundary wall, construction of cafe building, two elevational signs, external seating, car parking spaces, and ancillary site services and development works.	18/06/2020	430m
ABP	306903	Bollarney, Murrough, Wicklow, Co. Wicklow	Waste management facility and recycling centre, office building, car parking, wastewater treatment system, demolition of derelict building, construction of boundary walls, railings, and entrance gates.	08/04/2022	490m
WCC	19929	Brickfield Lane, Wicklow Town, Co. Wicklow	Four 2-storey detached dwellings and associated site works.	10/06/2020	730m
WCC	20534	Unit 3, The Murrough, Wicklow Town, Co. Wicklow	The development will consist of the following: the company proposes on accepting 900 tonnes (t) of Bulky Waste (Old Furniture) at its proposed facility per annum. A fraction of this waste furniture will be reworked/restored within the premises where viable, and the rest manually dismantled and sorted into the following different waste streams (i.e. wood, metal, plastic, residual waste). Each waste stream which arises during the dismantling of furniture will be sorted and deposited in a designated skip for that waste type. These skips will be contained inside the premises. Once a skip is filled, it will be collected and brought to an appropriate waste facility authorised to take that waste type. This activity constitutes a Material Change of Use of the site under the Planning and Development Act. This activity also constitutes a pre-treatment waste activity under the Waste Management (Facility Permit and Registration) Regulations.	02/09/2020	800m

Planning Authority	Ref. no.	Address	Summary of Proposed Development	Grant date	Distance from Site (approx.)
ABP	318978	Keatingstown Farm, Ballynerrin Townland, Wicklow Town, Co. Wicklow	Demolition of structures including a dwelling and farm sheds, construction of 13 no. houses, new road, footpaths, public lighting, open space, boundaries, landscaping, attenuation system, entrance location at existing farmyard entrance, and associated works.	Pending	880m
ABP	312888	Broomhall & Ballynerrin, Broomhall Court Road, Rathnew, Wicklow Town, Co. Wicklow	Construction of 66 no. dwellings.	05/05/2023	1.2km
ABP	312889	Townlands of Broomhall and Ballynerrin, Broomhall Court Road, Rathnew, Wicklow Town, Co. Wicklow	Construction of 84 no. dwellings.	03/05/2023	1.2km
WCC	24208	Merrymeeting View, (Townland of Merrymeeting), Rathnew, Co. Wicklow	To construct seven Social Housing Units and all associated works.	25/06/2024	1.3km
WCC	24263	Ballynerrin, (E.D. Wicklow Rural), Ballynerrin Lower, Co. Wicklow	The revision of and alterations to the planning permission previously granted under Planning File Register no. 21/58 which will consist of the following: <ol style="list-style-type: none"> 1. Revisions to the site boundaries and previously granted site edged red; 2. The omission of 12 no. 1-, 2- and 3-bed apartment units, and four 5-bedroom 2-storey terraced dwelling units, and in their place to construct the following: 3. Three ground floor 2-bedroom apartment units; 4. Three first and second floor 3-bedroom Duplex Units, 5. Seven 2-storey 3-bedroom terraced units, 6. 12 no. 2-storey 4-bedroom terraced units; 7. Connection to existing public utilities and services with ancillary changes to the previously granted infrastructure including the surface water drainage systems, foul water drainage systems and water main supply system; 8. Ancillary alterations to the existing roads, footpaths and public open spaces as previously granted under Planning File Register no. 21/58; 9. Connection to the existing and permitted roads and footpaths as granted under Planning File Register no. 21/58; 10. Revised boundary treatments, hard and soft landscaping; and 11. Ancillary works. 	05/09/2024	1.3km
WCC	201296	Ballynerrin (E.D. Wicklow Rural), Ballynerrin Lower, Co. Wicklow	92 dwelling units comprising four 4-bedroom detached, 35 no. 4-bedroom semi-detached, 25 no. 3-bedroom semi-detached, 12 no. 2-bedroom terrace, eight 3-bedroom duplex over 8 no. 2-bed apartments, the development of public parkland, together with ancillary site development works and services with part connection to sewers on adjoining Meadows Estate.	20/09/2021	1.4km
WCC	19138	Hawkstown Road, Burkeen, Wicklow Town, Co. Wicklow	74 no. dwelling units to be accessed from an existing exit/entrance off Hawkstown Road, Burkeen. The application consists of the following: three detached split-level 5-bedroom, one detached 4-bedroom, 12 no. semi-detached 4-bedroom, 12 no. semi-detached 4-bedroom, eight semi-detached 3-bedroom, 12 no. end of terrace 3-bedroom, 18 no. mid-terrace 3-bedroom, two semi-detached 2-bedroom, eight mid-terrace 2-bedroom, four end of terrace 1-bedroom units (ground floor), four end of terrace 1-bedroom units (first floor), a realignment of previously approved Reg. Ref. 17/1470 cycle and walkway route, together with required foul and surface water connections, attenuation tank, and all site works.	05/11/2019	1.6km

Planning Authority	Ref. no.	Address	Summary of Proposed Development	Grant date	Distance from Site (approx.)
WCC	191017	Knockrobin / Glebe, Rathnew, Wicklow Town, Co. Wicklow	3-storeys building as Phase 2 of the Wicklow Primary Healthcare Centre. The building will accommodate Tusla services, HSE mental health and social care services, primary care administrative offices, as well as ancillary accommodation including staff facilities, offices, meeting rooms, reception/waiting areas and plant rooms. Phase 2 will be accessed via the existing Phase 1 entrance and access road. The proposal will remove 83 no existing car parking spaces and provide 211 no. new car parking spaces. The application also provides for service compound, bicycle parking, landscaping and boundary treatments, and all ancillary site works and services.	06/11/2020	1.7km
ABP	308965	St. Eman's National School, Rathnew, Co. Wicklow	Demolition of school building and construction of 18 no. houses.	04/05/2021	1.8km
WCC	2158	Ballynerrin (E.D. Wicklow Rural), Ballynerrin Lower, Co. Wicklow	<p>Permission to erect a housing scheme which will consist of 95 no. residential dwelling units, which will comprise of the following:</p> <ol style="list-style-type: none"> 1. Two 2-storey (with dormer to second floor), 5-bedroom detached dwelling units; 2. Three 2-storey (with dormer to second floor), 5-bedroom semi-detached dwelling units; 3. 17 no. 3-storey, 4-bedroom terraced dwelling units; 4. Three 2-storey, 4-bedroom detached dwelling units; 5. Seven 2-storey, 4-bedroom semi-detached dwelling units; 6. 14 no. 2-storey, 3-bedroom semi-detached dwelling units; 7. Three 2-storey, 3-bedroom terraced dwelling units; 8. 22 no. ground floor single-storey (under duplex), 2-bedroom apartment units; 9. 22 no. first and second floor, 2-storey, 3-bedroom duplex apartment units; 10. 1 no. ground floor single-storey (under duplex), 1-bedroom apartment unit; 11. 1 no. first and second floor, 2-bedroom duplex apartment units; 12. Ancillary hard and soft landscaping; 13. Ancillary car parking and public lighting; 14. Connection to public services with associated attenuation systems; 15. Associated boundary treatments; 16. Connections and minor alterations to existing and permitted development granted under Planning File Register no. 17/1454, currently under construction, including pedestrian, vehicular, services and landscaping connection and assimilation points, with ancillary works; and 17. All of the above with ancillary works. 	06/10/2021	1.9km
WCC	211187	Broomhall & Ballynerrin, Broomhall Court Road, Rathnew Wicklow Town, Co. Wicklow	<p>Phase 2 of a 2-phase development and will consist of:</p> <ol style="list-style-type: none"> 1. The construction of 66 no. dwellings comprising one 3-bedroom bungalow, 18 no. 2-bedroom 2-storey houses, 44 no. 3-bedroom 2-storey houses, three 1-bedroom apartments in one 3-storey building; 2. All ancillary development works including footpaths, landscaping boundary treatments, public, private and communal open space areas (including balcony or terrace), car parking (112 no. spaces) and bicycle parking, single storey ESB substations, regrading/re-profiling of site where required, and all ancillary site development/construction works (including diversion of existing services and all new site services connections); and 3. Vehicular access from the existing Broomhall roundabout, via upgraded vehicular access route along the western side of development with pedestrian access to eastern boundary and to the north. <p>The proposed development will allow for all roads, services, and landscaping for Phase 1 and Phase 2 sites.</p>	02/02/2022	2.2km

Planning Authority	Ref. no.	Address	Summary of Proposed Development	Grant date	Distance from Site (approx.)
WCC	21411	Tinakilly, Rathnew, Co. Wicklow	The proposed development will consist of revisions to development permitted under WCC Reg. Ref. 17/219, ABP Ref. 301261-18, to provide for layout reconfiguration and replacement of 62 no. previously permitted units comprising four Type F, ten Type G, ten Type H, 27 no. Type J, three Type Ja, one Type K, one Type Ka, and six Type L, 3/4-bedroom detached and semi-detached 2/3-storey houses ranging from approximately 116m ² to 215m ² floor area each with 62 no. new houses comprising 12 no. 3-bedroom semi-detached 2-storey Type M1 house units (approximately 110m ² each), ten 3-bedroom semi-detached 2-storey Type N1 house units (approximately 113m ² each), three 3-bedroom semi-detached 2-storey Type N1a house units (approximately 114m ² each), one 3-bedroom semi-detached 2-storey Type N1b house unit (approximately 114m ²), 20 no. 4-bedroom semi-detached 2-storey Type 01 house units (approximately 134m ² each), two 4-bedroom semi-detached 2-storey Type 01A house units (approximately 136m ² each), ten 4-bedroom semi-detached 2-storey Type P1 house units (approximately 148m ² each), two 4-bedroom semi-detached 2-storey Type P1a house units (approximately 148m ² each), two 4-bedroom detached 2-storey Type P1B house unit (approximately 149 m ²). No additional houses are proposed under the subject application. All associated site development works, services provision, reconfiguration of internal access roads and footpaths to facilitate house type changes, associated amendments to boundary treatments, landscaping and car parking areas. All other site development works, services provision, vehicular and pedestrian access, landscaping and boundary treatment works will remain as permitted under WCC Reg. Ref. 17/219, ABP Ref. 301261-18, and WCC Reg. Ref. 20/1000. (See ABP Ref. no. 312888)	23/09/2021	2.2km
ABP	305460	Blackthorn Bay, Greenhill Road, Wicklow Town, Co. Wicklow	Six houses, access road, and associated site works.	14/01/2020	2.3km
WCC	211119	Townlands of Broomhall and Ballynerrin, Broomhall Court Road, Rathnew, Co. Wicklow	Proposal is for Phase 1 of a 2-phase development and will consist of: 1. The construction of 84 no. dwellings (and 2-storey creche approximately 301m ²) comprising one 3-bedroom bungalow, eight 2-bedroom houses, 61 no. 3-bedroom houses, seven 3-bedroom 'Courtyard' dwellings (all 2-storey), and one part 2-storey/3-storey 4-bedroom 'Courtyard' dwelling, six 1-bedroom apartments in two 3-storey buildings; 2. All ancillary development works include footpaths, landscaping boundary treatments, public, private and communal open space areas (including balcony or terrace), car parking (170 no. spaces) and bicycle parking, single storey ESB substations, regrading/re-profiling of site where required and all ancillary site development /construction works (including diversion of existing services and all new site services connections); and 3. Vehicular access from the existing Broomhall roundabout, via upgraded vehicular access route along the western side of development with pedestrian access to eastern boundary and to the north. The proposed development will allow for all roads, services and landscaping for Phase 1 and Phase 2 sites. (See ABP Ref. no. 312889)	02/02/2022	2.3km
WCC	19853	Broomhall Business & Enterprise Park, Merrymeeting, Co. Wicklow	Ground floor creche unit (576m ²), eight first-floor offices with communal meeting room and toilets (665m ²). The new building will be accessed via the existing park entrance road and together with all required new internal roads, car parking, outdoor crèche play area, connections to foul / surface water services, attenuation system, and site development works.	25/08/2020	2.4km
ABP	317895	Avonvale Manor, Ballynerrin Upper, Wicklow Town, Co. Wicklow	Modifications to approved residential development currently under construction: revisions to length of rear gardens of house no. 26 to 31 inclusive amended under Ref. no. 17/445 and 18/635, retention of recently constructed boundary wall at rear of houses no. 26 to 31 inclusive.	08/05/2024	2.4km
WCC	21663	Dominican College, Convent Road, Wicklow Town, Co. Wicklow	The construction of a 688m ² detached 2-storeys building to the south of the existing Dominican College building to include four classrooms with preparation rooms, toilet block, and ancillary accommodation. Connections are required to existing drainage, foul water, potable water and utility services with associated landscaping and site works. No material alterations are proposed to the Dominican College or the adjoining Dominican Convent or St. Dominic's Chapel which are Recorded Monuments (W14 to W19) and Protected Structures and all within a Zone of Archaeological Interest.	24/08/2021	2.5km
ABP	310552	Ballyguile Beg, Ballyguile Road, Co. Wicklow	Construction of 15 no. 4-bedroom detached dwelling houses.	29/08/2022	2.9km

Appendix C : Screening Checklist

Questions to be Considered	Yes/No/? - Briefly Describe	Is it Likely to Result in a Significant Effect? Yes/No/? - Why
1 Will construction, operation, decommissioning or demolition works of the Proposed Development involve actions that will cause physical changes in the locality (topography, land use, changes in waterbodies, etc.)?	Yes – the Proposed Development will introduce new infrastructure at the Wicklow Train Station and seeks to change the land use at the greenfield site to a car park and set-down facility.	No – no likely significant effects are likely to occur as a result of these changes given the scale and location of the Proposed Development, and the land use zoning at the greenfield site which indicates that these lands have been previously identified for development.
2 Will construction or the operation of the Proposed Development use natural resources such as land, water, materials or energy, especially any resources which are non-renewable or are in short supply?	Yes – the Proposed Development will likely require cut/fill material as well as a water supply during the construction phase.	No – given the size, type, and location of the Proposed Development, large amounts of natural resources are not anticipated to be required.
3 Will the Proposed Development involve the use, storage, transport, handling or production of substances or materials which could be harmful to human health, to the environment or raise concerns about actual or perceived risks to human health?	Yes – substances such as concrete, paint for road markings, oil, petrol, diesel, etc., will be required, mostly during the construction phase.	No – the Contractor's CEMP will include mitigation measures for the correct storage and usage of chemicals and materials which have the potential to cause harm to human health and/or the environment.
4 Will the Proposed Development produce solid wastes during construction or operation or decommissioning?	Yes – most waste generated by the Proposed Development will result from the construction phase. Waste generated during the operational phase will mostly comprise municipal waste associated with the operation of the Wicklow Train Station and occasional waste resulting from maintenance works.	No – given the type and scale of the Proposed Development, large quantities of waste are not anticipated to be produced. In addition, a RWMP will be produced by the Contractor for the Proposed Development and waste will be removed from the site by a licenced haulier to a licenced waste facility.
5 Will the Proposed Development release pollutants or any hazardous, toxic or noxious substances to air or lead to exceeding Ambient Air Quality standards in Directives 2008/50/EC and 2004/107/EC)?	Yes – mostly during the construction phase and in association with dust and GHG emissions from construction works and machinery/vehicles.	No – given the scale of the Proposed Development and the type of construction works required, dust and GHG emissions are unlikely to be significant and will be managed through the Contractor's CEMP. In addition, during the operational phase, the Proposed Development has the potential to reduce air emissions by encouraging a modal shift from private vehicles to active travel and public transport.
6 Will the Proposed Development cause noise and vibration or the releasing of light, heat energy or electromagnetic radiation?	Yes – noise and vibration emissions will mostly occur during the construction phase. During the operational phase, the Proposed Development will also result in a localised increase in light emission.	No – potential noise and vibration effects will be short-term and mostly limited to the construction phase, and will be managed through the Contractor's CEMP. Given the nature of the surrounding urban environment and the type and scale of additional lighting proposed, light emissions are unlikely to result in significant effects.
7 Will the Proposed Development lead to risks of contamination of land or water from releases of pollutants onto the ground or into surface waters, groundwater, coastal waters or the sea?	Yes – in the absence of mitigation measures, there is potential for water receptors in proximity to the site to be contaminated as a result of accidental releases of pollutants during the construction phase, or runoff of surface contamination associated with vehicle use of roads during the operational phase.	No – the Contractor's CEMP will include appropriate mitigation measures to avoid or reduce the risk of contamination of nearby water resources (including emergency response procedures for any leaks and spills and other measures such as silt fences). During the operational phase, surface runoff will be managed through the proposed drainage system which will be designed in accordance with relevant guidelines such as TII's DN-DNG-03022.

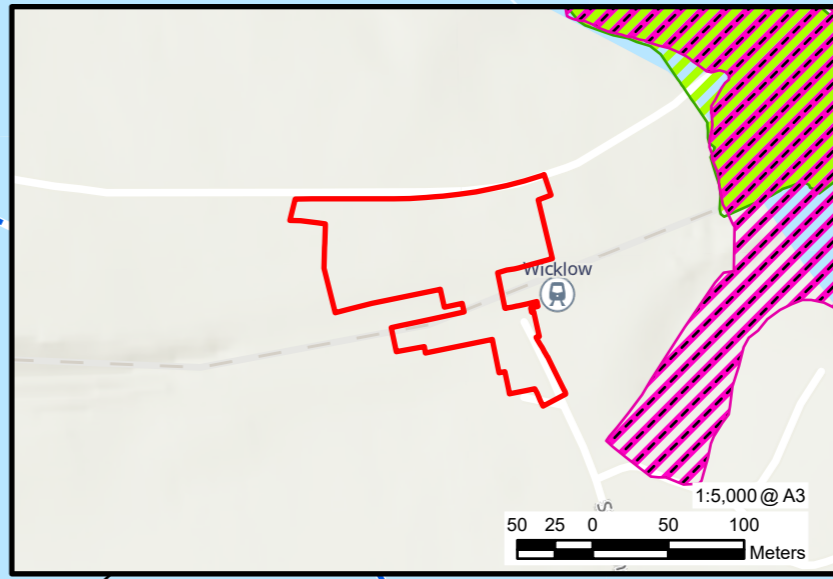
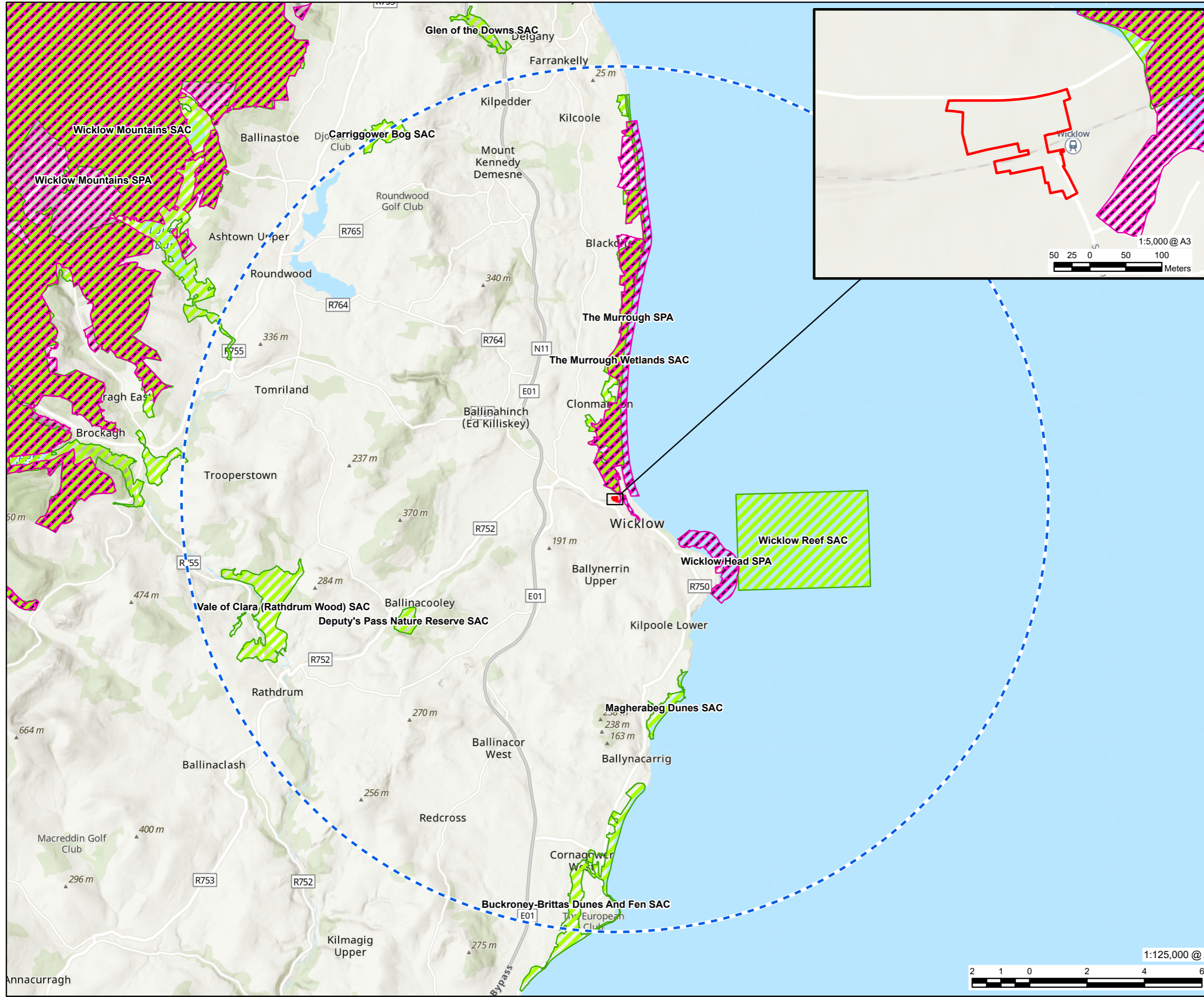
Questions to be Considered	Yes/No/? - Briefly Describe	Is it Likely to Result in a Significant Effect? Yes/No/? - Why
8 <i>Will there be any risk of accidents during construction or operation of the Proposed Development that could affect human health or the environment?</i>	Yes – during the construction and operational phases.	No – the risk of accidents occurring onsite will be managed through the Contractor's CEMP and CTMP during the construction phase, and mitigation measures embedded into the design of the Proposed Development as per relevant design standards and requirements during the operational phase (e.g., tactile paving for the safety of persons with visual disabilities).
9 <i>Will the Proposed Development result in environmentally related social changes, for example, in demography, traditional lifestyles, employment?</i>	Yes – the Proposed Development seeks to encourage a modal shift from private vehicles to active travel and public transport by introducing new pedestrian and cyclist facilities and providing new set-down facilities which will support the improvement of bus links.	No – the Proposed Development will not result in significant social changes.
10 <i>Are there any other factors that should be considered, such as consequential development, which could lead to environmental impacts or the potential for cumulative impacts with other existing or planned activities in the locality?</i>	Yes – there is potential for cumulative impacts between the Proposed Development and projects listed in Appendix B of this report, such as short-term impacts on the local road network and noise emissions.	No – taking into consideration that the majority of permitted developments within 1km of the site have finished construction, certain cumulative effects (e.g., noise emissions from multiple construction sites in proximity to each other) are unlikely to occur; should the construction phase of WCC Ref. no. 24252 overlap with that of the Proposed Development, this would be the only development likely to result in cumulative noise, traffic, and landscape and visual effects.
11 <i>Is the Proposed Development located within or close to any areas which are protected under international, EU, or national or local legislation for their ecological, landscape, cultural or other value, which could be affected by the Proposed Development?</i>	Yes – a wall within the curtilage of cultural heritage asset WTC-RPS 25 will be removed as part of the works proposed.	No – the AHA prepared for the Proposed Development has not identified significant effects associated with the removal of this wall. In addition, recommendations listed in the AHA Report will be incorporated into the design of the Proposed Development and construction requirements for the Contractor, to avoid or reduce the potential for adverse effects associated with impacts on the setting of cultural heritage assets.
12 <i>Are there any other areas on or around the location that are important or sensitive for reasons of their ecology e.g. wetlands, watercourses or other waterbodies, the coastal zone, mountains, forests or woodlands, that could be affected by the Proposed Development?</i>	Yes – the closest river waterbody is the Wicklow_010, located approximately 43m south. It discharges into Broad Lough, a transitional waterbody located approximately 34m east which is hydrologically connected to the Southwestern Irish Sea - Killiney Bay, located approximately 500m east.	No – at no point during the construction phase will water be abstracted or discharged to/from surface or ground waterbodies. The risk of pollution events will be managed through the Contractor's CEMP during the construction phase, and the proposed drainage system during the operational phase. Taking this into consideration as well as the type and scale of the Proposed Development, significant effects are unlikely to occur.
13 <i>Are there any areas on or around the location that are used by protected, important or sensitive species of fauna or flora e.g. for breeding, nesting, foraging, resting, overwintering, migration, which could be affected by the Proposed Development?</i>	Yes – Broad Lough Estuary forms part of international and national designations (the Murrough SPA, the Murrough Wetlands SAC, and the Murrough pNHA). In addition, the Wicklow Town Sites pNHA is located approximately 307m east.	No – as concluded in the AA Screening prepared for the Proposed Development, no significant effects on designated sites are likely to occur.
14 <i>Are there any inland, coastal, marine or underground waters (or features of the marine environment) on or around the location that could be affected by the Proposed Development?</i>	Yes – the Southwestern Irish Sea - Killiney Bay is located approximately 500m east of the site.	No – no significant effects on waterbodies hydrologically linked to the Southwestern Irish Sea - Killiney Bay are likely to occur. Taking this into consideration as well as the type of Proposed Development, no significant effects on this coastal waterbody are likely to occur.

Questions to be Considered	Yes/No/? - Briefly Describe	Is it Likely to Result in a Significant Effect? Yes/No/? - Why
15 <i>Are there any areas or features of high landscape or scenic value on or around the location which could be affected by the Proposed Development?</i>	No – the site is within an urban environment and there are no sensitive landscape designations in or in close proximity to the site.	No – landscape and visual effects associated with the presence of construction works and machinery will be short-term while permanent loss of green spaces and trees/scrub will be required. During the operational phase, given the urban context, design considerations aimed at ensuring that the Proposed Development is appropriate to the existing setting, and the type and scale of the Proposed Development, no significant landscape and visual effects are likely to occur.
16 <i>Are there any routes or facilities on or around the location which are used by the public for access to recreation or other facilities, which could be affected by the Proposed Development?</i>	Yes – the Proposed Development is within the Wicklow Train Station, adjacent to the East Glendalough School, and in proximity to recreational facilities, a riverwalk, and civil service buildings.	No – the Wicklow Train Station will remain operational during the construction phase, with the exception of one weekend during which works over the railway line will be completed. Access to Station Road and walking trails will be maintained. In addition, a CTMP will be prepared and implemented by the Contractor to avoid or reduce traffic nuisances and ensure the safety of all road users. During the operational phase, the Proposed Development will improve access to these sites by providing a new link between lands north and south of the railway line, and has the potential to reduce congestion in the area.
17 <i>Are there any transport routes on or around the location that are susceptible to congestion or which cause environmental problems, which could be affected by the Proposed Development?</i>	Yes – Station Road is susceptible to congestion as a result of traffic coming to and from the aforementioned facilities in Response 16.	No – refer to Response 16.
18 <i>Is the Proposed Development in a location in which it is likely to be highly visible to many people?</i>	Yes – given its location in an urban setting and the Wicklow Train Station.	No – as per Response 15, no significant landscape and visual effects are likely to occur.
19 <i>Are there any areas or features of historic or cultural importance on or around the location that could be affected by the Proposed Development?</i>	Yes – refer to Response 11.	No – as per Response 11, no significant effects on cultural heritage assets are likely to occur.
20 <i>Is the Proposed Development located in a previously undeveloped area where there will be loss of greenfield land?</i>	Yes – the site includes greenfield land north of the railway line.	No – as per Response 1, no significant effects are likely to occur as a result of developing this greenfield land.
21 <i>Are there existing land uses within or around the location e.g. homes, gardens, other private property, industry, commerce, recreation, public open space, community facilities, agriculture, forestry, tourism, mining or quarrying that could be affected by the Proposed Development?</i>	Yes – the site is within an urban environment with mix-uses in the surrounding environs which include private property, civil service buildings, commercial properties, and educational and recreational facilities, amongst others.	No – the Proposed Development requires limited change to existing land uses which involves land take requirements within green spaces associated with the eastern boundary of the East Glendalough School. The Applicant is engaged in land acquisition discussions with the relevant landowners who have expressed their support for the Proposed Development. Access to and from surrounding properties will be maintained throughout the construction phase.
22 <i>Are there any plans for future land uses within or around the location that could be affected by the Proposed Development?</i>	Yes – lands north of the railway line within the greenfield site are zoned for 'Enterprise and Employment' while lands within the East Glendalough School are zoned for 'Community/Educational/Institutional'.	No – the use of lands adjacent to train stations for their improvement is in line with the vision and objectives of the CDP, and the location of the Proposed Development will also support other CPOs which aim to improve accessibility to schools and public transport, particularly via active travel.

Questions to be Considered	Yes/No/? - Briefly Describe	Is it Likely to Result in a Significant Effect? Yes/No/? - Why
23 <i>Are there areas within or around the location which are densely populated or built-up, that could be affected by the Proposed Development?</i>	Yes – the Proposed Development is situated within an urban environment.	No – as per Responses 5, 6, 16, 21, and 22, no significant effects are likely to occur.
24 <i>Are there any areas within or around the location which are occupied by sensitive land uses e.g. hospitals, schools, places of worship, community facilities, that could be affected by the Proposed Development?</i>	Yes – the Proposed Development is adjacent to the East Glendalough School, and in proximity to recreational facilities and civil service buildings.	No – as per Responses 5, 6, 16, 21, and 22, no significant effects are likely to occur.
25 <i>Are there any areas within or around the location which contain important, high quality or scarce resources e.g. groundwater, surface waters, forestry, agriculture, fisheries, tourism, minerals, that could be affected by the Proposed Development?</i>	Yes – there are river and transitional waterbodies and designated sites in proximity to the site (refer to Responses 12 and 13), and the site extends over a 'Poorly productive bedrock' ground waterbody.	No – as per Responses 7 and 13, no significant effects are likely to occur.
26 <i>Are there any areas within or around the location which are already subject to pollution or environmental damage, e.g., where existing legal environmental standards are exceeded, that could be affected by the Proposed Development?</i>	Yes – Broad Lough and the Wicklow ground waterbody are 'At Risk' of not meeting WFD objectives.	No – as per Responses 7 and 12, no significant effects are likely to occur.
27 <i>Is the Proposed Development location susceptible to earthquakes, subsidence, landslides, erosion, flooding or extreme or adverse climatic conditions e.g. temperature inversions, fogs, severe winds, which could cause the Proposed Development to present environmental problems?</i>	Yes – a limited area on the southernmost section of the site along Station Road has 'Moderately Low' landslide susceptibility. There are areas susceptible to experience flooding approximately 38m south of the site.	No – taking into consideration the location of the site and that GI have informed the design of the Proposed Development, landslides and flooding are not considered significant environmental problems for the Proposed Development. In addition, as surface runoff will be managed through the proposed drainage design, the Proposed Development is unlikely to result in a significant increase in the risk of flooding in nearby lands.
28 <i>Summary of features of Proposed Development and of its location indicating the need for EIA.</i>	No likely significant effects were identified during the EIA screening process and a full EIA culminating in the preparation of an EIAR is not required.	

Source: European Commission's 'Environmental Impact Assessment of Projects: Guidance on Screening' (EC, 2017)

Appendix D : European Sites Figure



PROJECT
Wicklow Port
Access Road Link

CLIENT
Wicklow County Council

CONSULTANT
AECOM
Adelphi Plaza,
George's Street Upper
Dun Laoghaire, Co Dublin,
A96 T927
T +353-1-969-6220

LEGEND

- Site Boundary
- 15km Study Area
- Special Areas of Conservation (SAC)
- Special Protected Areas (SPA)

NOTES

Esri, Intermap, NASA, NGA, USGS, Esri Community Maps Contributors, Esri UK, Esri, TomTom, Garmin, Foursquare, GeoTechnologies, Inc, METI/NASA, USGS, Esri UK, Esri, TomTom, Garmin, Foursquare, GeoTechnologies, Inc, METI/NASA, USGS, Sources: Esri, Airbus DS, USGS, NGA, NASA, CGIAR, N Robinson, NCEAS, NLS, OS, NMA, Geodatastyrelsen, Rijkswaterstaat, GSA, Geoland, FEMA, Intermap and the GIS user community

ISSUE PURPOSE
FOR ISSUE
PROJECT NUMBER
60647343
FIGURE TITLE
European Sites

FIGURE NUMBER
Figure 1

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